

E. Architectural and Site Design Guidelines

1. Introduction

a. Purpose and Intent

These guidelines are intended to create high quality appearance, to assure compatibility, to direct character and form, and to enhance the industrial/business park's overall value. They are intended to promote visual and spatial harmony between individual buildings and the entire Borel Airpark Center and to create the highest quality product in this market segment in the vicinity.

The purpose of these guidelines is to encourage creativity, imagination and a high level of harmony and consistency within the Center. Special emphasis has been placed upon methods that tend to reduce large-scale visual impact of buildings and to balance the elements of structure, landscaping and paving.

These guidelines define the development controls which are provided for in the specific plan for the Borel Airpark Center. The developer of each parcel shall thoroughly familiarize him/herself with the intent and requirements of the specific plan, guidelines, Airport Land Use Commission's recommendations and all applicable governmental codes and regulations.

All development plans, landscaping plans, specifications and graphic designs shall be submitted to an Architectural Review Committee as described and provided for in the guidelines. The Architectural and Development Guidelines are intended to supplement the specific plan as an aid in the submittal of plans for approval by providing more detailed information on which to base a review. In the event a conflict arises between the provisions within these guidelines and the specific plan, the provisions within the specific plan shall prevail. Approval is to be deemed final; authorizing the commencement of any such construction, improvement or alterations.

b. Objectives:

- o To create a high quality business complex which is thoughtfully planned and compatible with the local environment.
- o To reduce any negative visual impact of large scale buildings by encouraging tasteful and imaginative designs for individual buildings.
- o To obtain a consistency of basic architectural elements while encouraging each future property owner to explore the creative site planning and architectural possibilities of the site.

c. Architectural Review and Control

Development of the Borel Airpark Center project will be subject to a two-tiered control system for architectural related factors. The first layer of architectural control is established with the current property owners who will establish review authority over the architectural appearance of all construction with the project. The current property owners are in the process of forming a Property Owners Association (POA), which will be responsible for making project-wide decisions and establishing CC&R's and an Architectural Review Committee (A.R.C.).

The second level of architectural review will be codes, covenants and restrictions (CC&R's) which will be established with each tentative tract map. These will be established as a recorded instrument and will carry with the property regardless of whether the current owner or merchant builders are the actual developers. CC&R's will be administered by the POA's A.R.C. which will be established in accordance with regulations established within the CC&R's.

2. Site Planning Guidelines

a. General

Each of the building projects within the Borel Airpark Center is an integral part of the total community. As such, it is critical that

each individual building project interact with the others to produce a cohesive environment. To accomplish this goal, strict adherence to the basic relationships shown in the specific plan will be required of all developers (see Exhibits 36 through 39). In addition, each building project will be required to comply with the following criteria.

The following criteria are intended to create a business complex composed of general office/professional uses, commercial, manufacturing, distribution, storage, sales operations, aeronautic and industrial uses, where each site has its own identity while maintaining an overall master planned business park appearance.

- o Site entrance and building entries shall be readily identifiable and accessible to the first time visitor.
- o Conflicts between automobiles, service vehicles and pedestrians within the site shall be minimized.
- o The developer shall integrate existing master planned landscaped areas into their individual site plans in a well designed manner.
- o The following items shall be screened from public view:
 - * Service areas
 - * Trash areas
 - * Mechanical and electrical equipment
 - * Solar panels
 - * Parking lots
 - * Exterior storage of materials and products
- o Exterior enclosures, structures and walls shall be compatible and coordinated with the architectural character of the buildings.

b. Pedestrian Circulation

On-site pedestrian walkways should be fully integrated with the internal site vehicular circulation system to allow safe and

convenient pedestrian access through parking areas and from perimeter street walkways to building entrances.

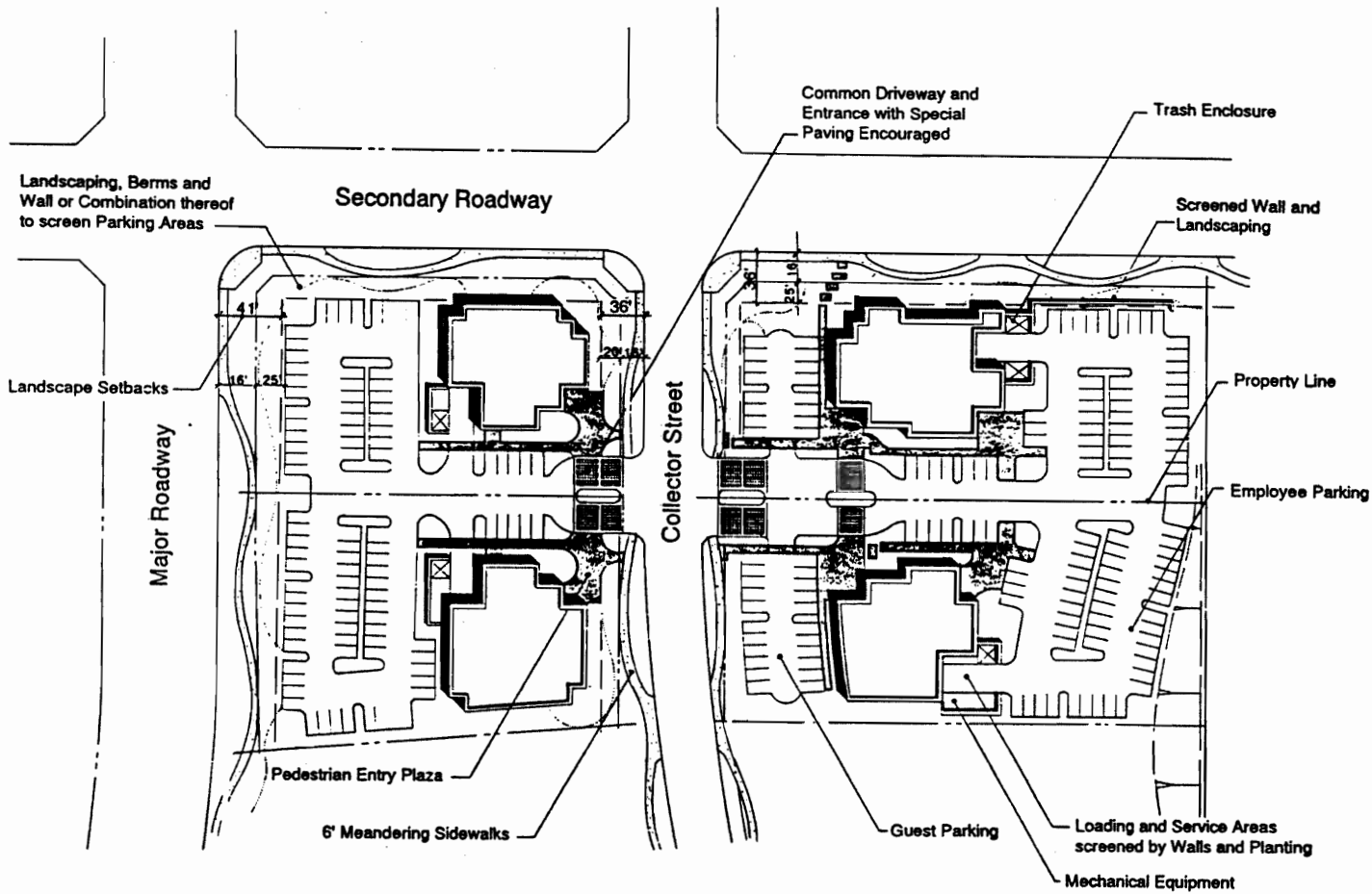
Ample pedestrian walkways should be provided throughout parking areas with special consideration given toward walkway landscaping, lighting and amenities. Pedestrian walks that cross parking aisles and driveways should be emphasized by a contrasting paving material.

c. Parking and Circulation

Parking space requirements and parking design layouts within the Borel Airpark Center shall provide sufficient off-street parking and loading spaces to adequately meet the demand, as well as ensure safe, easily maintained off-street parking facilities. Off-street parking standards and requirements shall conform to Riverside County Land Use Ordinance No. 348, Section 18.12, and the following:

- o All parking areas and drives shall be separated from landscaped areas by concrete curbs. Parking areas shall be paved with impervious materials including asphalt and concrete.
- o It shall be the responsibility of the developer and/or owner to ensure that all parking areas and drives are properly lit and maintained for safe operation of vehicles. Large expanses of parking area should be discouraged. Parking areas should be dispersed into convenient clusters.
- o Parking areas should be set back a minimum of six feet (6') from buildings, providing ample landscaped area adjacent to the building.
- o Designated "cycle parking" areas and bicycle racks should be considered for the convenience of employees using bicycles and motorcycles. In order to avoid the clutter of cycles parked in unplanned locations, it is recommended that planned "cycle parking" areas be conveniently located to employee entrances and be provided with racks having security locking capabilities.

Typical Office Sites



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CENTER



Not to Scale

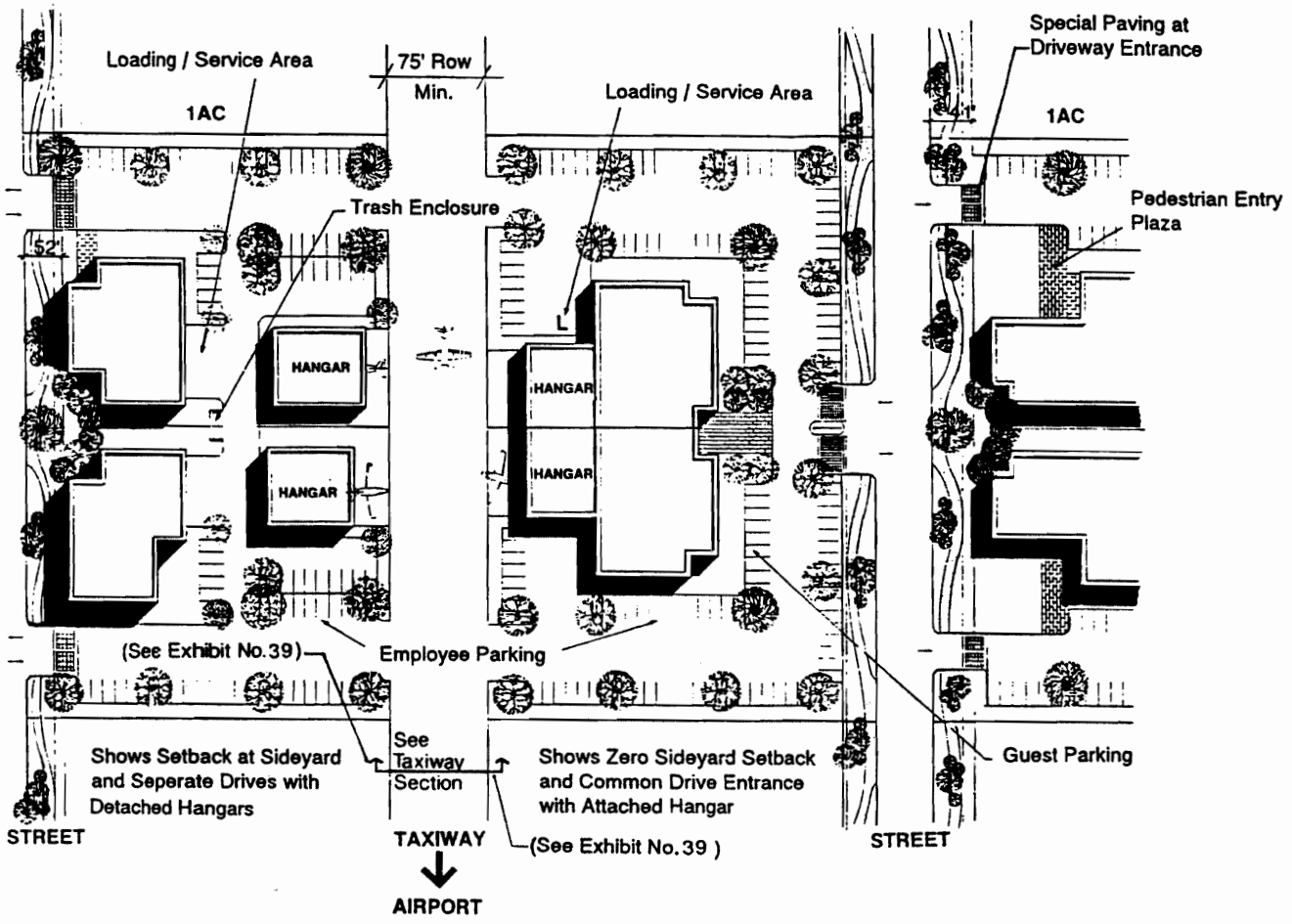
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Exhibit No.: 34

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Not To Scale

Industrial Sites with Taxiways

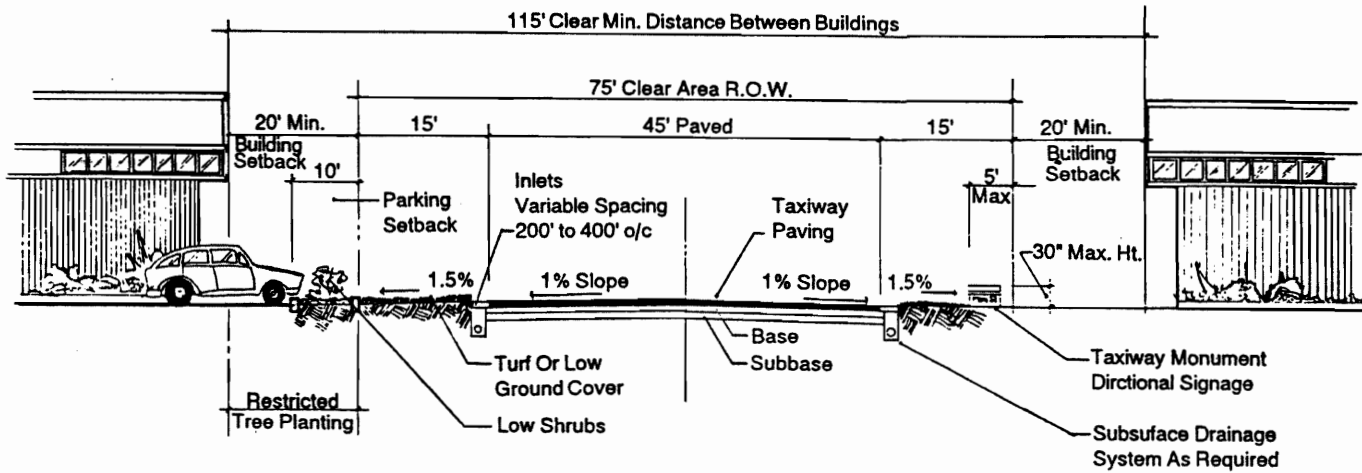
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Taxiway Section



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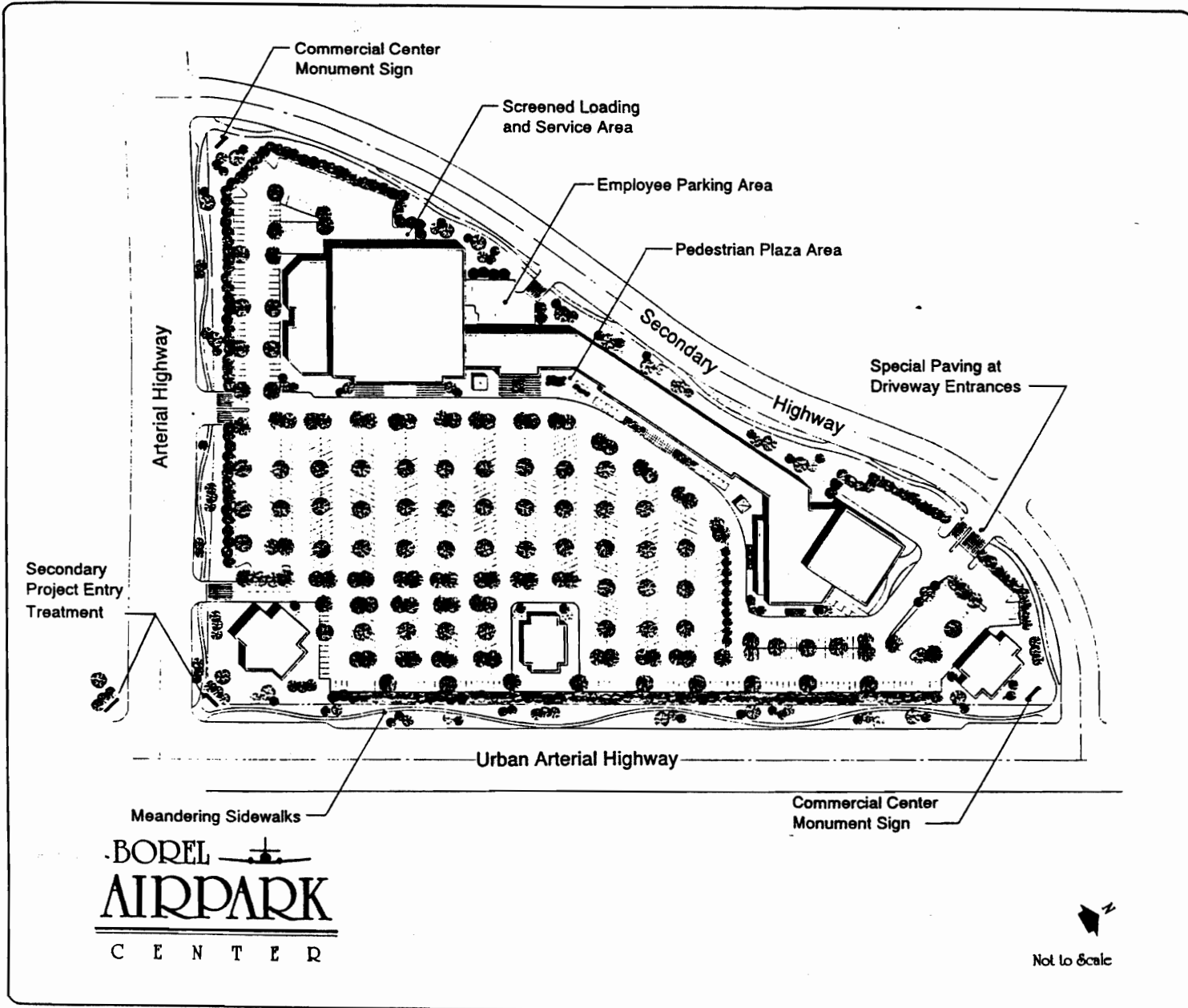
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Exhibit No. 36

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Typical Commercial Site

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Exhibit No.: 37

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- o Access drives should be coordinated with adjacent lots so as not to impede the efficient flow of peak period traffic.
- o Access drive designs should incorporate provisions for efficient vehicle stacking during peak periods of use.
- o Access drives should have adequate separation for safety purposes adjacent to the public right-of-way.
- o All driveway entrances should be emphasized by a contrasting material located within the project boundaries, not within that portion of the County right-of-way and the front yard setback, and shall be designed in accordance with the requirements of the County of Riverside.

d. Loading and Service Areas

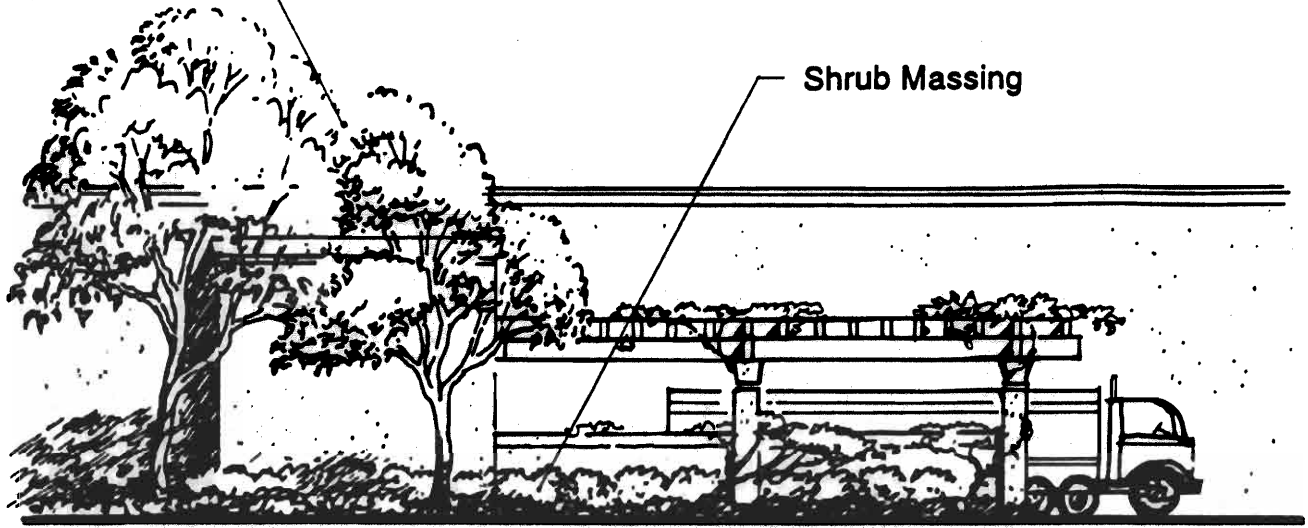
Sufficient loading and unloading spaces shall be provided for each retail, office/commercial, business park and public development as required per the County of Riverside's standards. Adequate provisions and space shall be made for maneuvering freight vehicles and for the handling of freight, independent of the public street system. No on-street vehicle loading shall be permitted (see Exhibit No. 40). In addition, all loading areas shall be located as follows:

- o Loading facilities shall be set back, recessed or screened from view by walls, berms or landscape plantings or a combination thereof so as not to be visible from adjacent parcels and streets.
- o For lots bound by public streets, loading will be permitted only along the elevations at the sides of the building facing interior property lines unless said loading space is screened by walls, fences or landscaping.
- o All loading facilities bordering residential development shall have a six foot (6') high solid concrete or masonry wall constructed on the property line.

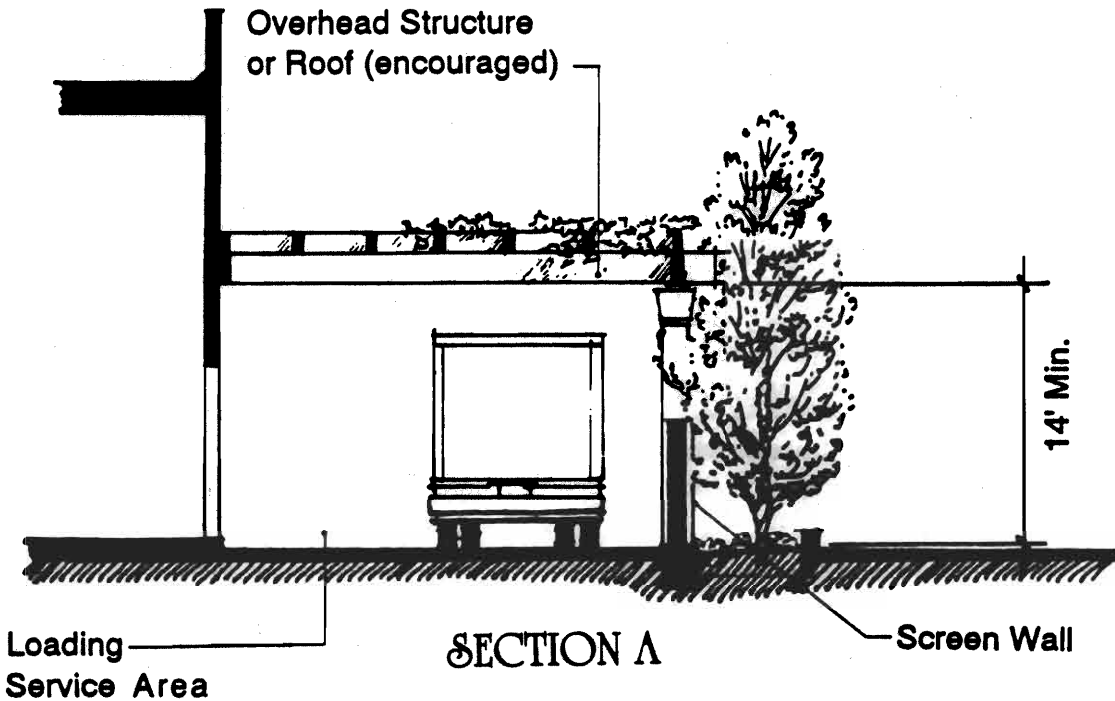
Loading and Service Areas

Tree Groupings

Shrub Massing



ELEVATION



Loading Service Area

SECTION A

Screen Wall

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Exhibit No.: 38

o Buildings and structures shall be designed and situated on the lot and loading facilities shall be constructed and located such that motor vehicles may be located or unloaded at any loading dock, door or loading area without impairing the flow of traffic on adjacent streets.

e. Environmental Art Program

The Architectural Review Committee (ARC) strongly encourages each developer to establish a program for the placement of outdoor environmental art within the individual building project. The environmental art could be sculptures, fountains, monumental plazas or landscaped gardens. It is recommended that by nature of this project, the art develop and maintain an industrial and/or aeronautical theme. The environmental art shall be designed in scale with the surrounding buildings and structures and located adjacent to or within public means; i.e., along streets and walks or within parking areas, courtyards and plazas. The ARC feels that a reasonable budget for such an art program would be one half of one percent of the total construction cost of the building project. As with any element of design at the Borel Airpark Center, outdoor environmental art must be reviewed and approved by the ARC before its installation.

f. Building Setbacks

Building setback requirements for industrial, office and commercial uses shall conform to Riverside County Land Use Ordinance No. 348 and these guidelines. Where conflicts between the guidelines and the zoning ordinance exist, the more restrictive standard shall apply.

g. Landscape Setbacks

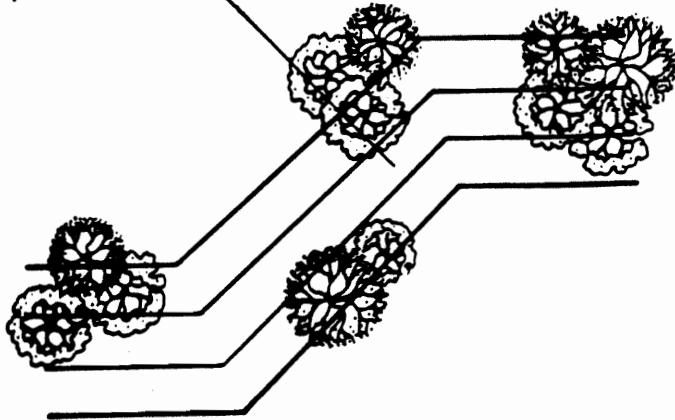
Please refer to Section E. 5., Landscape Design Guidelines

h. Grading

All grading within the Borel Airpark Center project area shall be required to conform to all applicable Riverside County ordinances and

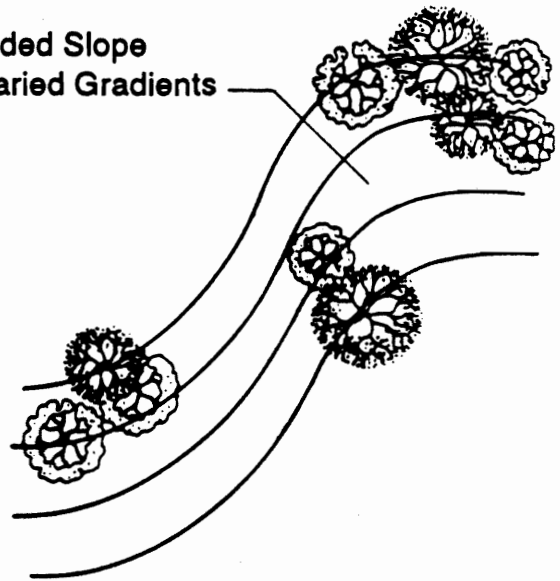
Landscape Grading

Standard Engineered
Slope Bank

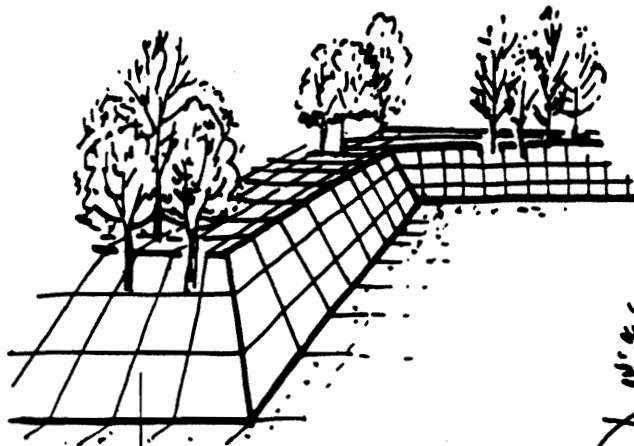


UNACCEPTABLE

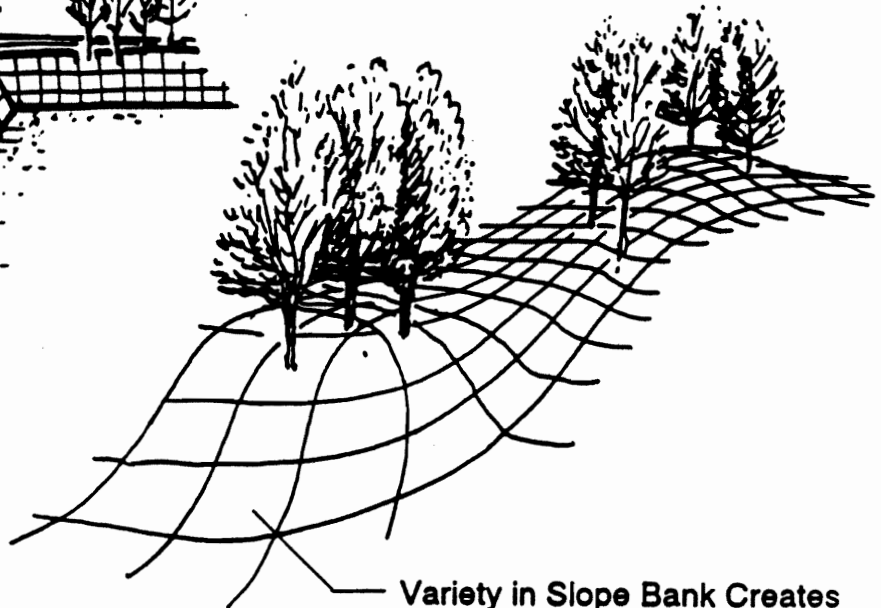
Contour Graded Slope
Bank with Varied Gradients



ACCEPTABLE



Avoid Steep, Sharp Cuts
and Fill that Create Unnatural
Appearance



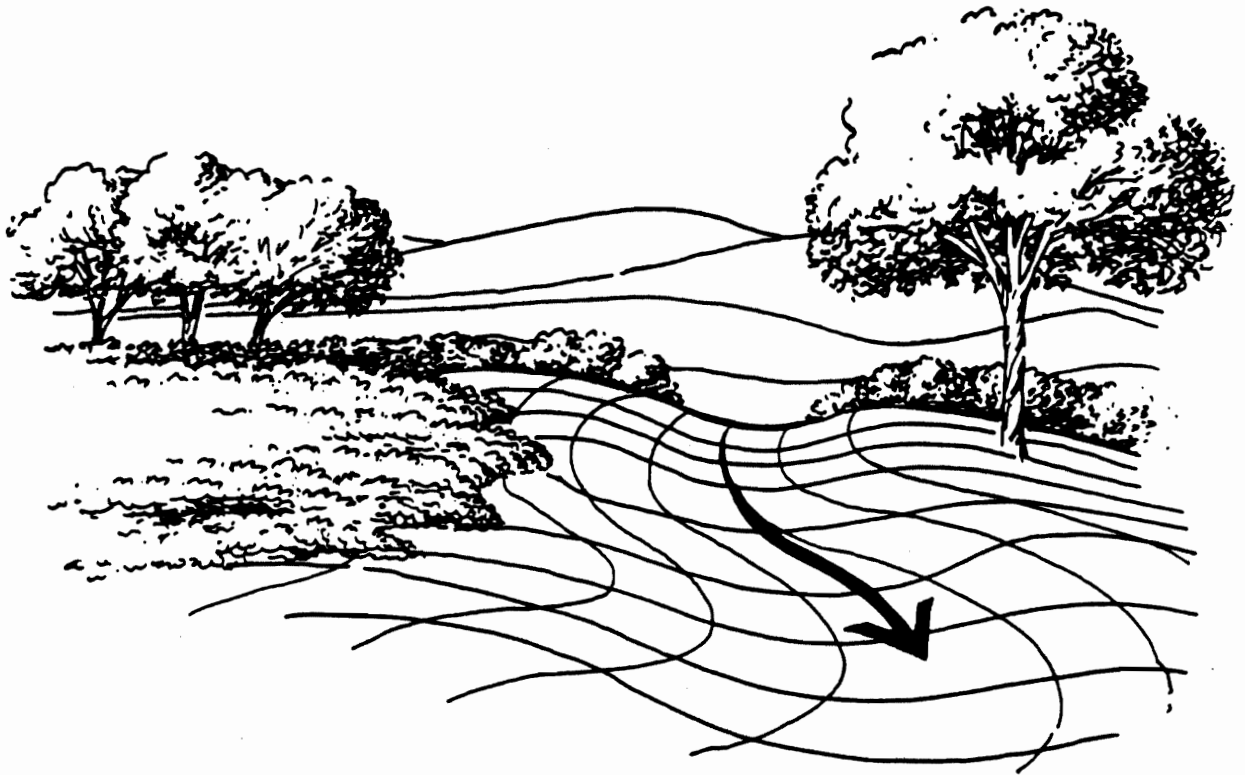
Variety in Slope Bank Creates
Natural Appearance

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Exhibit No.: 39

Landscape Drainage Swales



Design drainage swales to use the natural surface runoff for plant irrigation, minimize engineered look.

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Exhibit No.: 40

standards. It is recommended that specific grading consideration be given to the natural gentle rolling, semi-arid landscape of the Borel Airpark Center in order to preserve and protect sensitive hillsides and slopes, as well as enhance open space and drainage courses (see Exhibit No. 41).

The maximum slope gradient is 2:1. The maximum slope of any turf areas shall be 4:1. All grading will be consistent with the Master Plans of Grading, Drainage and Utilities.

Whenever possible, slopes should be graded to blend into the contours of existing natural slopes.

All new slopes shall be properly stabilized by planting with vegetation to prevent erosion and shall comply with the landscape section of these guidelines.

- o Earth berms should be rounded and natural in character.
- o Driveways should not exceed twelve percent (12%) slope, parking lots should not exceed ten percent (10%) slope. Paved areas should follow existing grades as much as possible to minimize grade differences at their periphery.

Natural floodways and drainage channels may not be built upon and shall be designated as permanent open space. Drainage courses, natural floodways and greenbelts should be enhanced with landscaping or riding and hiking trails whenever feasible.

Manufactured slopes adjacent to natural open space should be planted with naturalized drought resistant materials.

Contour grading and variable slope gradients along the open space areas will be encouraged.

i. Drainage

It is important to consider the drainage from the site. Natural conditions of drainage should be preserved where possible.

Large bench drains and concrete swales are to be avoided. It is preferable to use contoured swales planted with riparian vegetation. Underground pipes and gutters should be routed to energy dissipators to reduce the possible effects of erosion. Exhibit No. 42 shows drainage swales to be used at the project.

Drainage of all roof areas shall be connected directly to off-site storm drains where possible. In the event no storm drain system exists, roof drains should flow into an on-site subterranean drainage system which discharges off-site or at curb face of parking lots. Sheet flow of water across entry drive-ways is not allowed.

j. Garden Walls and Fences

Walls and fences within the Borel Airpark Center shall be utilized to provide security, reinforce screening requirements for adjacent land uses, or act as a contributing site element to an overall landscape theme for a specific parcel. Walls shall be constructed of masonry, concrete or stone and shall be consistent in treatment with adjacent buildings and structures. Please refer also to Section 3.5(k) for project walls. See Exhibit No. 43 for Typical Walls/Offsets and Planter Pockets.

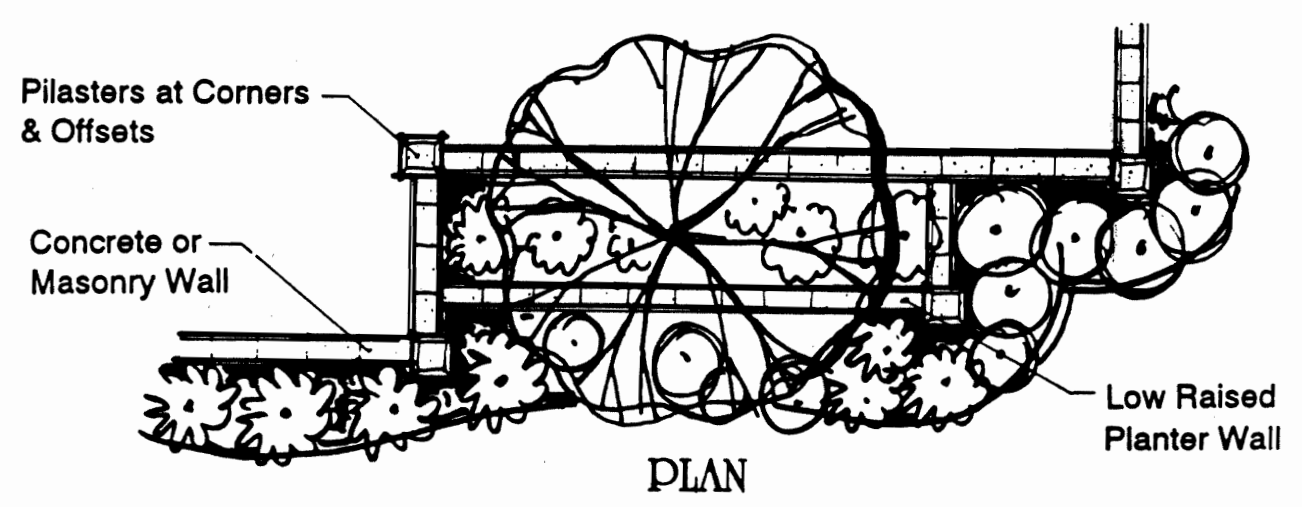
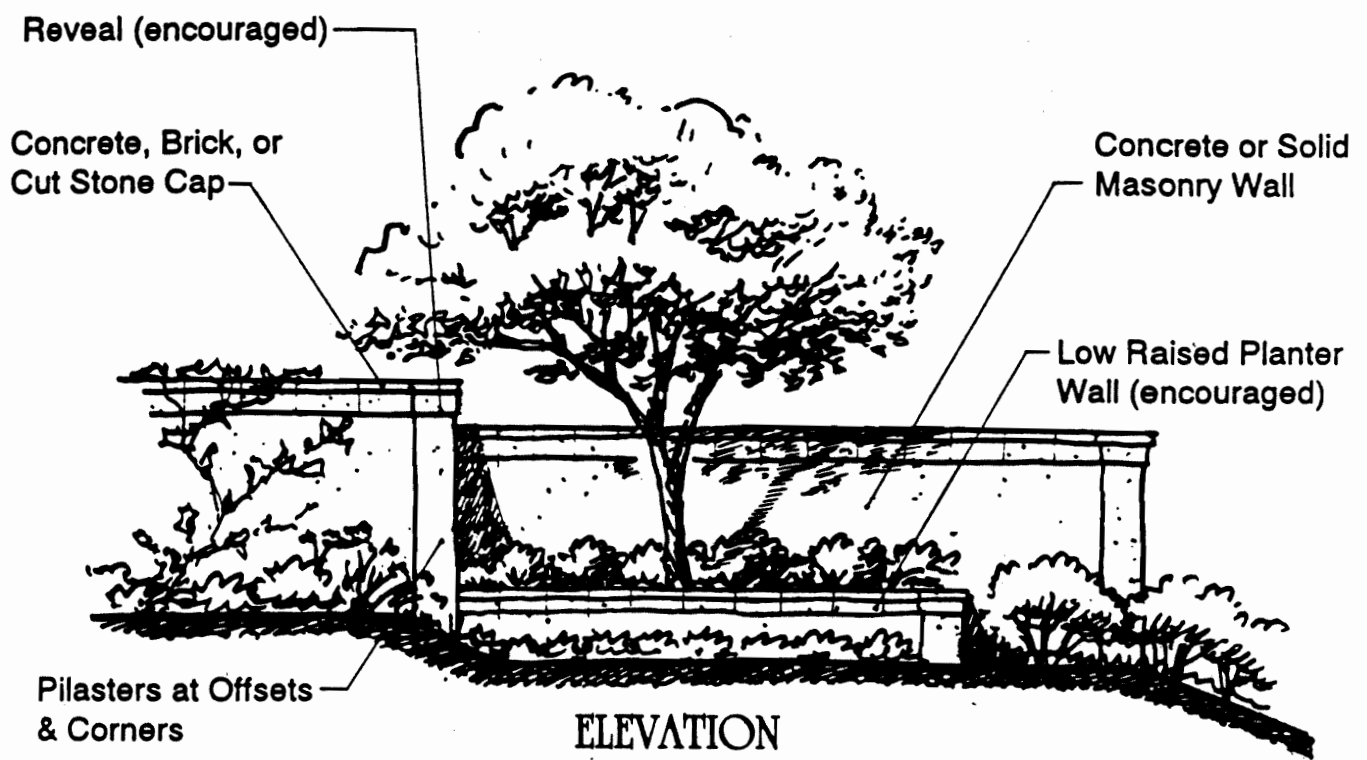
Long, unbroken, solid wall lines are monotonous and are discouraged. Walls should provide offsets every fifty feet (50'), or include recessed planting areas to increase visual interest and appeal, or include a combination of differing materials and textures.

Walls used for screening shall be a minimum of six feet (6') in height. Permanent chain link fencing is discouraged and shall require the discretionary approval of the Architectural Review Committee (ARC).

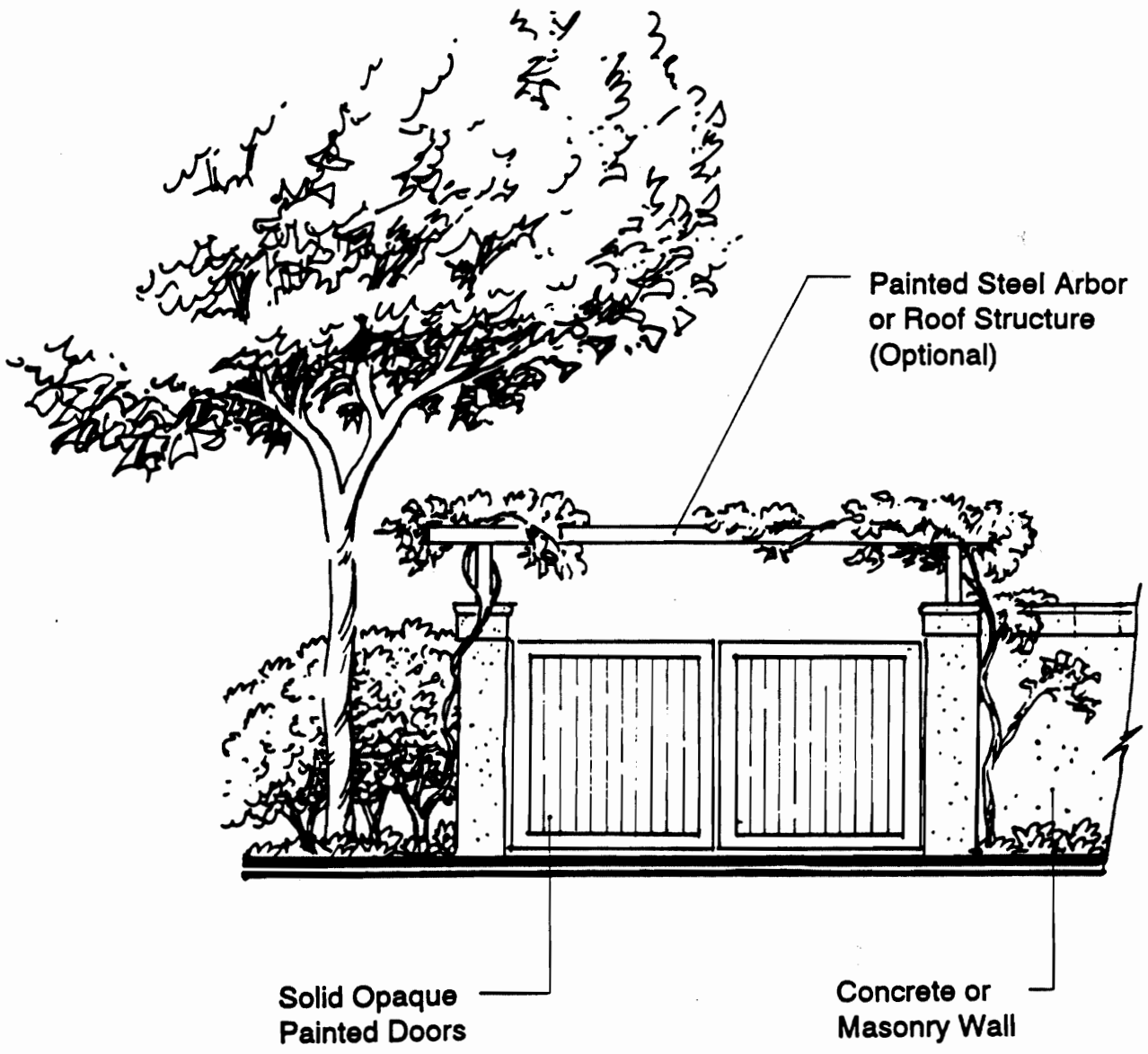
k. Utilities

All utility lines shall be installed underground. No permanent overhead lines on site will be permitted. No transformer pads shall be permitted in the front yard landscaped setback areas. Where transformer pads

Typical Walls / Offsets and Planter Pockets



Trash Enclosure



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Exhibit No.: 42

may be seen from any street, they will be screened with landscaping of an appropriate height or walls similar in treatment to adjacent buildings.

All other above ground utility appurtenances, regardless of the location on the parcel, shall be screened with landscape planting.

1. Outdoor Storage, Service Areas and Trash Enclosures

Outdoor storage areas and trash enclosures shall be located upon the site so as not to be visible from primary visual exposures such as streets, pedestrian and vehicular entry areas, recreational and open space amenities (see Exhibit 44).

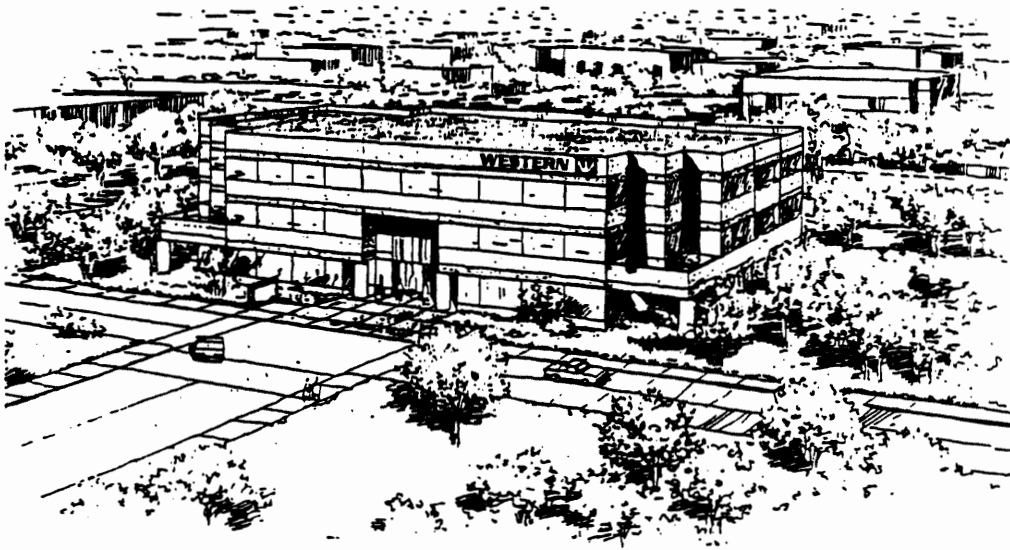
No materials, supplies or equipment, including trucks or motor vehicles, shall be stored upon a site except inside a closed building or behind a visual barrier screening such materials, supplies or vehicles from adjacent sites so as not to be visible from neighboring properties and streets. No materials, supplies or equipment shall project above the screened height of the storage area. No outdoor storage areas may be located within any required front yard areas.

In all portions of the project area, outdoor storage areas should be located in inconspicuous areas such as the side or rear portion of a site. No outside storage shall be permitted within any required setback.

Outdoor storage areas may be visually screened by using landscape plantings, walls, earth berms or any combination thereof.

Service areas and trash enclosures shall be screened from off-site views by solid masonry or concrete enclosures of at least six feet (6') high with opaque gates constructed of wood, metal or similar low maintenance materials. Service areas and trash enclosures shall be conveniently accessible to the buildings they are designed to serve. Service areas and trash enclosures shall be located in such a manner as to minimize noise and odor nuisance.

Offices



Acceptable



Unacceptable

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