

with the intent of achieving design continuity throughout the project. These guidelines were also developed in accordance with the design objectives of the Specific Plan.

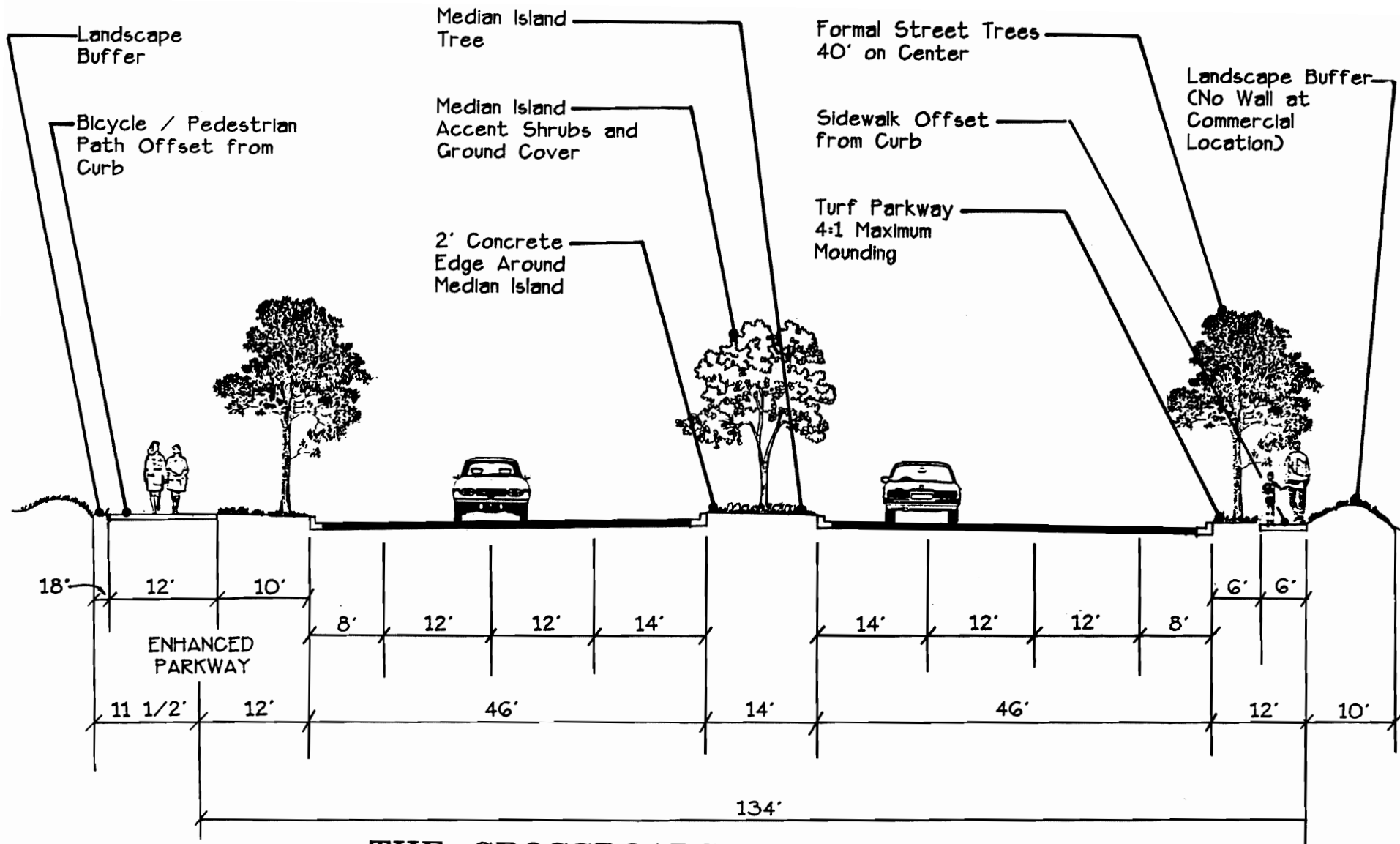
## **1. Roadway Landscape Guidelines**

The roadway landscape design guidelines detailed in this section establish a reference for the hardscape and planting of public right-of-ways and common areas. The guidelines include a description and illustrative plans which reflect the quality and image which The Crossroads Specific Plan wishes to establish. The guidelines recognize and encourage the use of landscaping in complementing and enhancing project architecture. Primary importance to the landscape development character is the creation of a community theme that reinforces several goals: traffic safety, functional circulation, and a definition of neo-traditional patterns through the incorporation of broad boulevards, well-shaded pedestrian walkways, and paseos that connect transit stops and commercial areas to the residential uses.

A special note on trees planted in the roadway landscape zone is made: shade trees provide comfort for the pedestrian and are key to creating an accommodating environment for pedestrians or bicyclists in the climate of Riverside County. Reduction of heat build-up from unshaded asphalt, a cooler micro-climate from evapotranspiration, and protection for groundcover, turf or immature plantings below the shade line are critical benefits from proper placement of shade trees. The developer shall to give extraordinary attention to the placement and care of the roadway landscape tree plantings.

### **a. Newport Road Right-of-Way and Landscape Zone (see Figure IV-29)**

The landscape development associated with the Specific Plan roadway along Newport Road has been designed to provide for a strong sense of community character and for a functional pedestrian corridor. Newport Road is proposed as an urban arterial highway with a 134 ft. right-of-way and a raised, landscaped median, and is planned to serve the heavy pedestrian and bicycling activity of the neighborhood commercial center. Newport Road also will carry the majority of commuting traffic through the Specific Plan, and should be planted with a limited number of the same species of trees, so that a unified and distinctive quality image is portrayed.



THE CROSSROADS IN WINCHESTER  
 SPECIFIC PLAN

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FIGURE

NEWPORT ROAD  
 134' RIGHT OF WAY

IV-29

In consideration of these facts, the design criteria calls for the following roadway landscape:

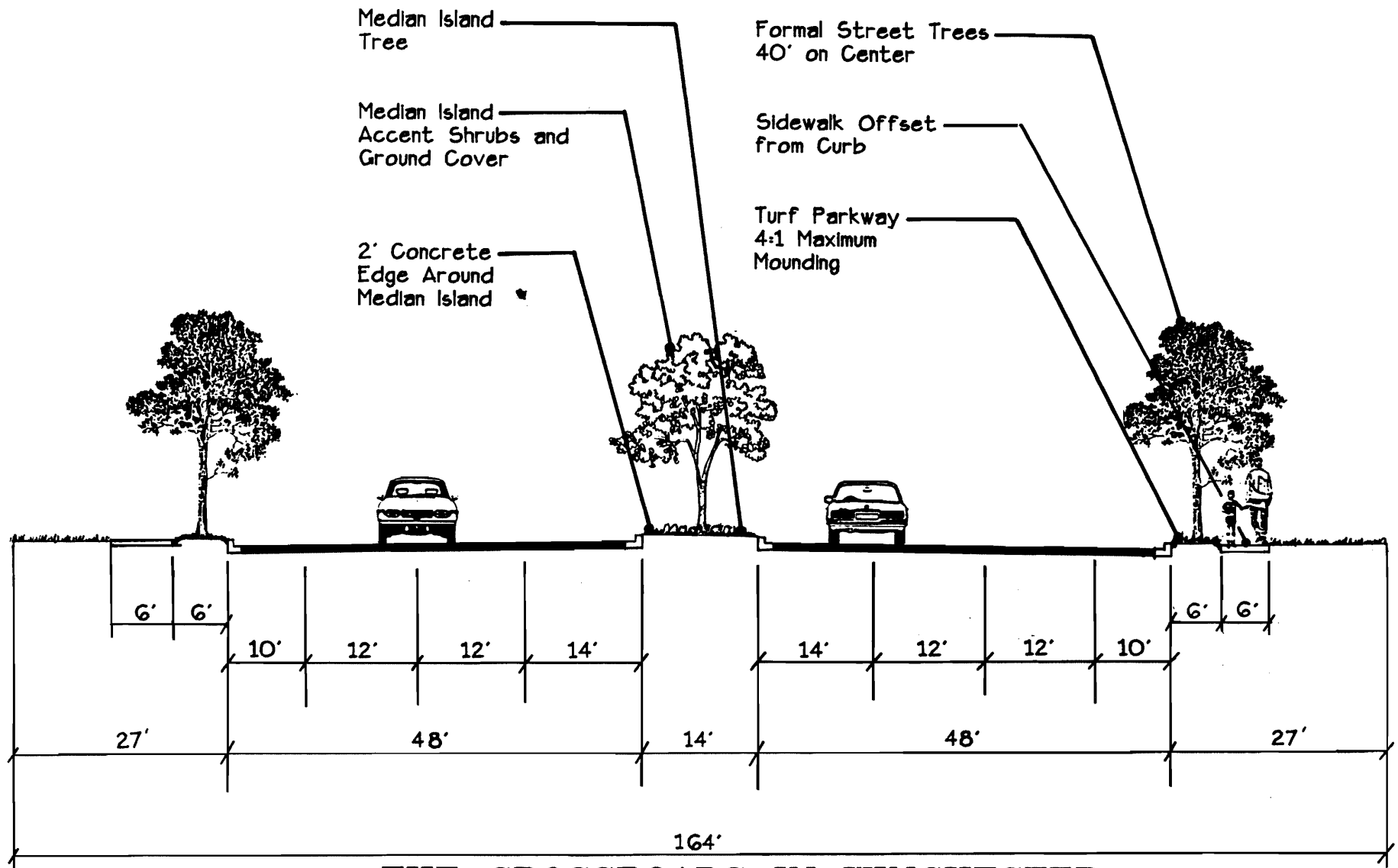
1. Median Island Tree planting with shade trees in a formal, repeating design between 40 and 60 feet on center
2. Median Island Accent Shrubs and Ground Cover (see Figure IV-32a)
3. Parkway on south side of the street: 10-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 40 feet on center, enhanced bike/pedestrian walkway, 12-foot wide concrete, paralleling and adjacent to turf parkway, landscape buffer 18 inches, at outside of enhanced sidewalk, with low shrubs or groundcover. Parkway on north side of the street: 6-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 40 feet on center and low shrubs or groundcover, concrete sidewalk 6-foot wide, paralleling and adjacent to turf parkway; optional landscape buffer at outside of sidewalk, with low shrubs or groundcover with 4:1 maximum mounding. The alternative is placement of the community wall at the outside edge of the sidewalk.
4. Total horizontal measurement of roadway landscape is 145.5 feet, extending 11.5 feet, beyond County of Riverside Ordinance standard.

**b. Highway 79 (Winchester Road) Right-of-Way and Landscape Zone (see Figure IV-30)**

The landscape development associated with Highway 79 shall be in accordance with the guidelines which CALTRANS has developed for State highways. Total right-of-way, width of median island, distance between trees, parkway width and other guidelines are highlighted and illustrated in Figure IV-30.

Where feasible and within the limitations set by CalTrans, hardscape and landscaping enhancements to Highway 79 have been outlined herein to create a visual statement for the entry into a master planned community, as well as to buffer both visual and audible traffic byproducts of the highway from the residential areas adjacent to it.

Highway 79 is situated at the eastern boundary of The Crossroads, and is proposed as an urban arterial highway with a 164 ft. right-of-way and a raised, landscaped median. The west side of the roadway landscaping lies within The Crossroads Specific Plan; the east side of Highway 79 lies outside of the Specific Plan and, while landscaping is suggested, implementation will rely on future development of the east side of the roadway.



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FIGURE

WINCHESTER ROAD  
164' RIGHT OF WAY

IV-30

In consideration of these facts, the design criteria calls for the following roadway landscape:

1. Median Island Tree planting with shade trees in a formal, repeating design between 40 and 60 feet on center
2. Median Island Accent Shrubs and Ground Cover
3. Parkway on west side of highway: 6-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 40 feet on center, turf planting with 4:1 maximum mounding, concrete sidewalk 6 feet wide, paralleling and adjacent to turf parkway, landscape buffer 10 feet wide, at outside of sidewalk, with low shrubs or groundcover.
4. Total horizontal measurement of roadway landscape on west side, extending from centerline of the roadway, is 82 feet, consistent with County of Riverside Ordinance standard.

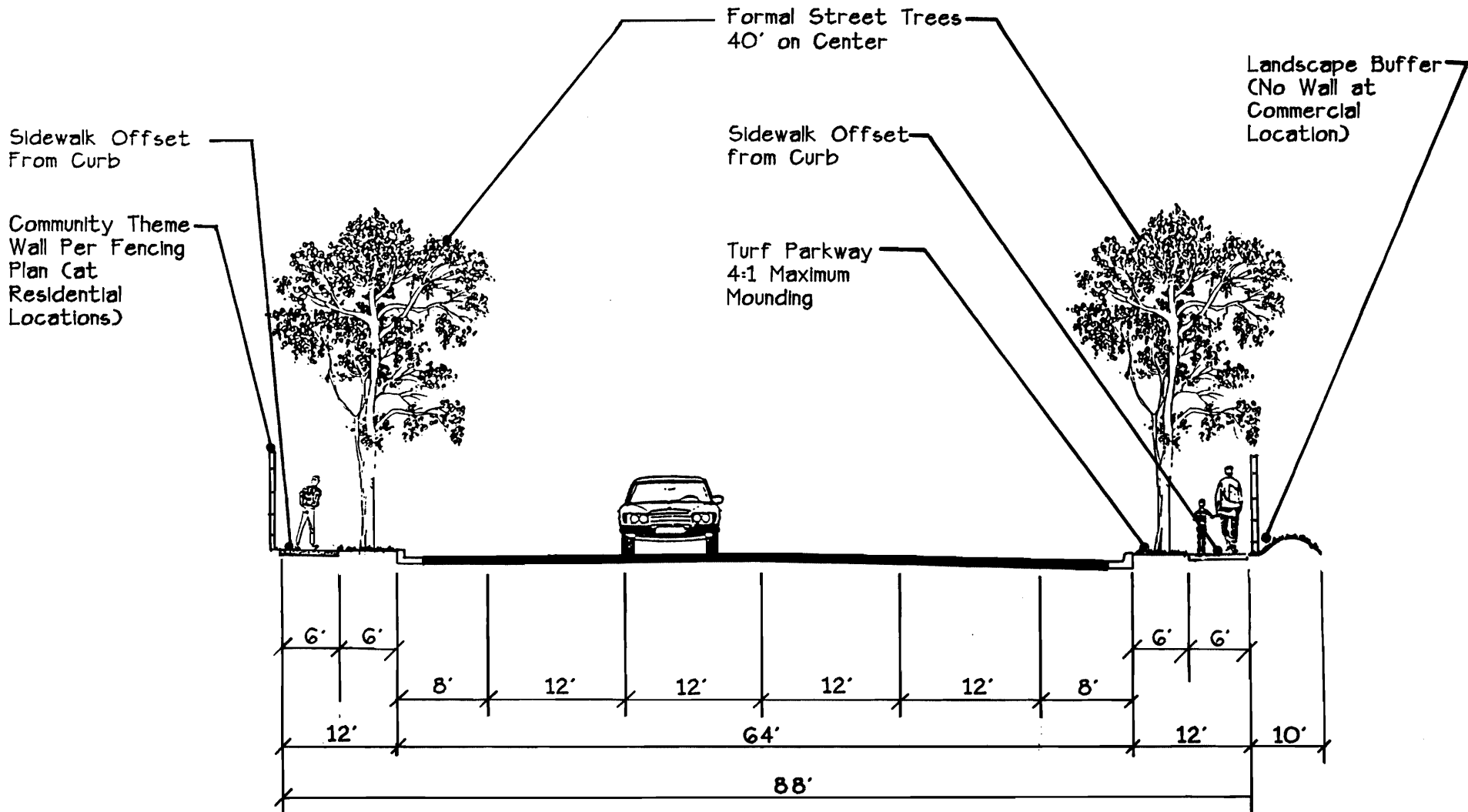
**c. Rice Road Right-of-Way and Landscape Zone (see Figure IV-31)**

The landscape development associated with the Specific Plan roadway along Rice Road has been designed to provide for continuity with the design of the arterial roadways within the Specific Plan. Rice Road is proposed as a secondary highway with an 88-foot right-of-way, is a minor crossing over the Salt Creek Channel, and is planned to serve the residential neighborhoods in the northern part of the Specific Plan. The east side of the roadway lies within The Crossroads Specific Plan; the west side of Rice Road lies outside of the Specific Plan, and landscaping and implementation will rely on future development to the west of the roadway. In consideration of these facts, the design criteria calls for the following roadway landscape:

1. Parkway on west side of street: 6-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 40 feet on center and low shrubs or groundcover, concrete sidewalk 6-foot wide, paralleling and adjacent to the turf parkway; optional landscape buffer at outside of sidewalk, with low shrubs or groundcover with 4:1 maximum mounding. The alternative is placement of the community wall at the outside edge of the sidewalk.

**d. Old Newport Road Right-of-Way and Landscape Zone (see Figure IV-31)**

The landscape development associated with the Specific Plan roadway along the Old Newport Road alignment has been designed to provide for continuity with the design of the arterial roadways within the Specific Plan. Old Newport Road



## THE CROSSROADS IN WINCHESTER SPECIFIC PLAN

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FIGURE

OLD NEWPORT ROAD  
 RICE ROAD  
 88' RIGHT OF WAY

IV-31

exists at the writing of this guideline as an unpaved lane, and is shown as an arterial with a 100-foot right-of-way in the County of Riverside Master Plan of Highways. No proposals to utilize it as a throughput road exist, however, due to the realignment of East Newport Road some 2,000 feet to the north; therefore, Old Newport Road is proposed in this Specific Plan as a secondary highway with an 88-foot right-of-way, to provide access to the most southerly planning area of The Crossroads, as well as to various parcels to the south of the Plan. The north side of Old Newport Road lies within the Plan, while the south side lies outside the boundary of the Plan. In consideration of these facts, the design criteria calls for the following roadway landscape:

1. Parkway on north side of street: 6-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 40 feet on center and low shrubs or groundcover, concrete sidewalk 6 feet wide, paralleling and adjacent to turf parkway; optional landscape buffer at outside of sidewalk, with low shrubs or groundcover with 4:1 maximum mounding. The alternative is placement of the community wall at the outside edge of the sidewalk.

**e. "A" Street Right-of-Way and Landscape Zone (see Figure IV-32)**

The landscape development associated with the Specific Plan roadway along "A" Street has been designed to provide for continuity with the design of the arterial roadways within the Specific Plan. "A" Street is proposed as Collector with a 66-foot right-of-way, and is planned to serve the pedestrian and bicycling activity of the residential neighborhood centers. Because "A" Street is the north-and-south backbone of the Plan, execution of hardscape and landscape along their edges is extremely crucial in encouraging alternate modes of transportation within the Plan. The streets shall reflect a design which provides for safety to pedestrians and bicyclists, well-developed shade patterns, easy and well-lit access to the two parks and the commercial center, and a pleasing visual presentation through the use of quality landscape materials and coordinated flora. In consideration of these facts, the design criteria calls for the following roadway landscape:

1. Parkway on west side of the street: six-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 30 feet on center; enhanced bike/pedestrian walkway, 12-foot wide concrete, paralleling and adjacent to turf parkway; landscape buffer 18 inches, at outside of enhanced sidewalk, with low shrubs or groundcover.

Parkway on east sides of the streets: six-foot wide turf section, paralleling and adjacent to the curb, with formal street trees placed 30 feet on center and low shrubs or groundcover; concrete sidewalk 6 feet wide, paralleling and adjacent to turf parkway; optional landscape buffer at outside of sidewalk, with low shrubs or

groundcover with 4:1 maximum mounding. The alternative is placement of the community walk at the outside edge of the sidewalk.

2. Total horizontal measurement of roadway landscape is 76.5 feet, extending 11.5 feet beyond County of Riverside Ordinance standard.
3. Access openings to cul-de-sacs or street right-of-way within the residential areas adjacent to the roadway, to provide more direct access within the Specific Plan. Access openings shall contain minimum landscape and hardscape design integrated with the community wall, if present, at each opening location.

## **2. Community Entry and Perimeter Elements**

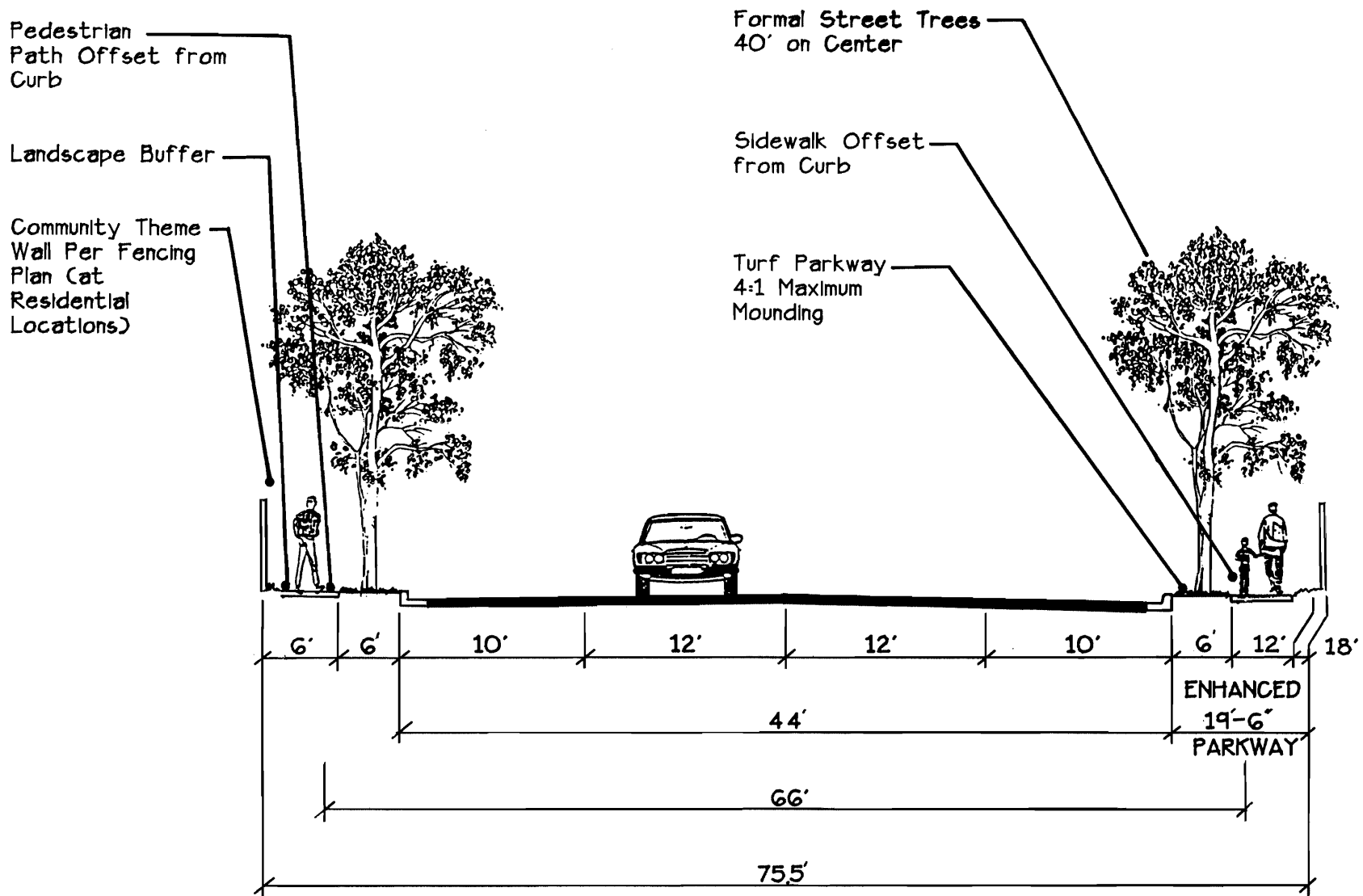
Specifically addressed in this section are the guidelines for major and minor community entries, community nodes and neighborhood entries. A distinctive community character is defined through the use of the prescribed hard and softscape materials at strategic points of the community. The landscape parameters establish a reference for the planting of public areas and entries, using the plant palette described in Section IV.D.4.d "Plant Material Guidelines".

Community hardscape elements at The Crossroads are generally composed of entry monumentation, perimeter walls and fencing. Hardscape elements contribute to the functioning of the community by identifying the development with a landscape hierarchy, giving the homeowner a sense of place and belonging.

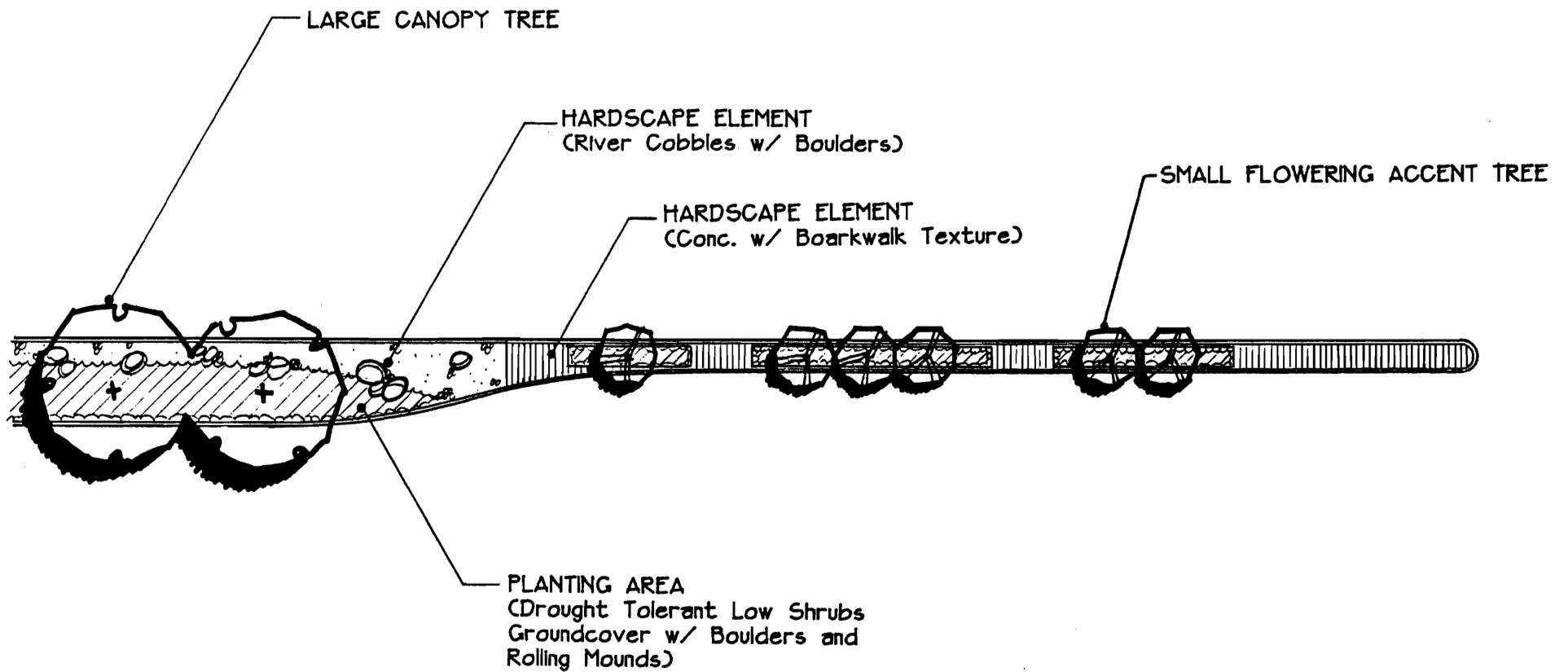
### **a. Entry Monumentation**

Entry monumentation consists of a thematic blend of construction materials, landscaping features and identifying signage which provide strong landmarks to reinforce the rural nature and community character of the Winchester area. Visible landmarks also help orient pedestrians and make walking routes interesting and memorable. A hierarchy of theme entries has been designed which consists of the following:

- Major Community Entry Monumentation
- Minor Community Entry Monumentation
- Neighborhood Entry Monumentation
- Commercial Entry Monumentation



THE CROSSROADS IN WINCHESTER  
 SPECIFIC PLAN



## THE CROSSROADS IN WINCHESTER SPECIFIC PLAN

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TYPICAL MEDIAN  
LANDSCAPING

FIGURE

IV-32A