

C. Architecture - Layout and Structure Massing

The Crossroads in Winchester Specific Plan is a small-scale community which incorporates the general guidelines of the Transit-Oriented Development (TOD) concept mentioned previously in this document. While not strict in its interpretation of the rules which govern land planning and pedestrian movement in a TOD, The Crossroads nevertheless has been planned to accommodate the pedestrian as much as possible, and to diminish sole reliance on travel via the automobile, in accordance with the following statement about TOD design:

The TOD mixes residential, retail, office, open space and public uses within comfortable walking distance, providing options for residents and employees to travel by transit, bicycle, or foot, as well as by car. (Calthorpe Associates)

As implementation of the Plan proceeds from a macro-environment (land planning in Section IV.B) to a micro-environment, the elements which comprise the master developer's and builder's design must be consistent with each other as well as with the overall vision. The subsections of this section guide the design, in an increasing detail, from streets, to lot layout, to building massing and, finally, to the community elements of entries, signage, lighting, enclosures, etc.

1. Street Layout - Pedestrian-Oriented

a. Arterial Streets and Thoroughfares

Arterial streets and thoroughfares should allow efficient conveyance of through traffic. The arterial may not serve as a significant barrier to pedestrian and bike activity - such a situation will increase the number of daily vehicle trips. Convenient pedestrian and bike crossings shall be provided wherever cross-arterial connections are made. Large, traffic-carrying roadways should be located at the periphery of a TOD.

b. Street Pattern

The TOD street system should be clear, formalized and interconnected, converging to the transit stop and commercial center. Cul-de-sac streets should be avoided, or modified to allow pedestrians and bicyclists to pass through.

Utilizing guiding principles of the 1991 Sacramento County General Plan Update, The Crossroads intends to communicate to the developer/merchant builder the aspects of TOD which must be implemented in the basic roadway design. The guiding principles to be applied are as follows:

- Link land use with existing or proposed transit alignments
- Reduce the number of auto trips and regional Vehicle Miles Traveled (VMT)

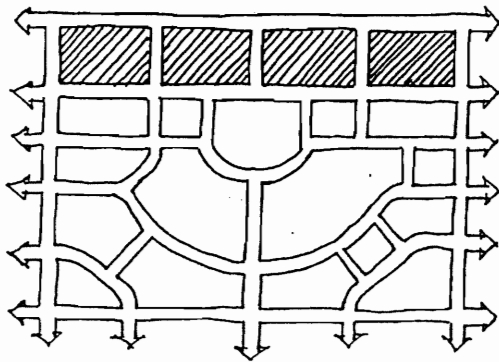
- Reduce air pollutant emissions
- Provide a diversity of housing types
- Design the urban area efficiently

Appropriate

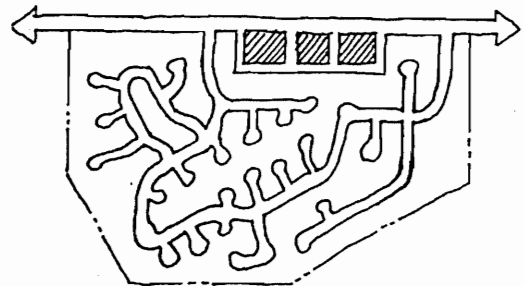
- Encourage the layout of grid streets, or modified grid, feeding into the collector streets, to encourage direct access to recreation and shopping areas. Where cul-de-sacs are used, provide openings from cul-de-sac onto collector streets to avoid circuitous routes to points of interest
- Interconnected street system
- Simple and memorable, with landmarks
- Provide multiple and parallel routes
- Provide the shortest and most direct path to destinations
- Security through access and visibility

Inappropriate

- Winding roads, dead-end streets, and cul-de-sacs
- Collector streets overburdened by excessive traffic
- Street pattern which is circuitous, or complex patterns that will discourage pedestrians
- Busy, smoggy, wide and "unfriendly" boulevards
- Isolation from rest of community



Appropriate



Inappropriate

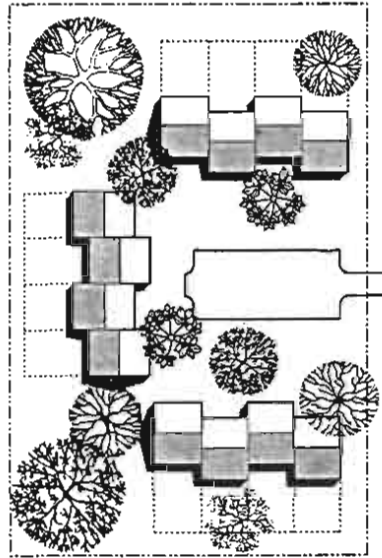
2. Lot Layout

a. Layout Styles

The layout of individual lots is governed by the density proposed in that planning area, having taken into account the variables which impact development: topographical and environmental characteristics, integration into surrounding uses, efficient use of the land and, in this Specific Plan, accommodation of pedestrian and bike travel.

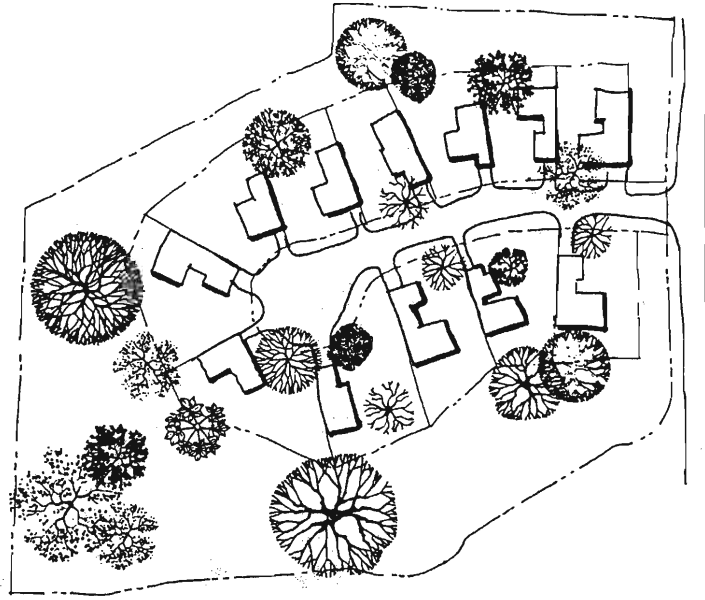
Many varied approaches to lot layout are acceptable, when the principles and intent of the TOD concept are respected. The following examples highlight the different possibilities; all are appropriate when a continuous pedestrian and bicycle system is incorporated within and between neighborhoods.

Private Courtyard



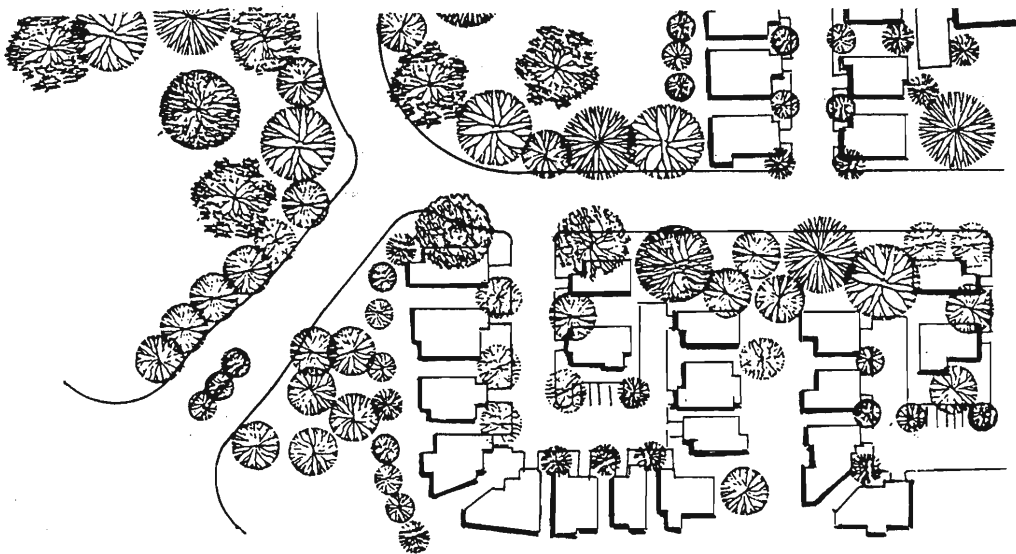
Private courtyard with shared access reduces overall street miles and places more residential units in close proximity to services

Zero Lot Line Configuration

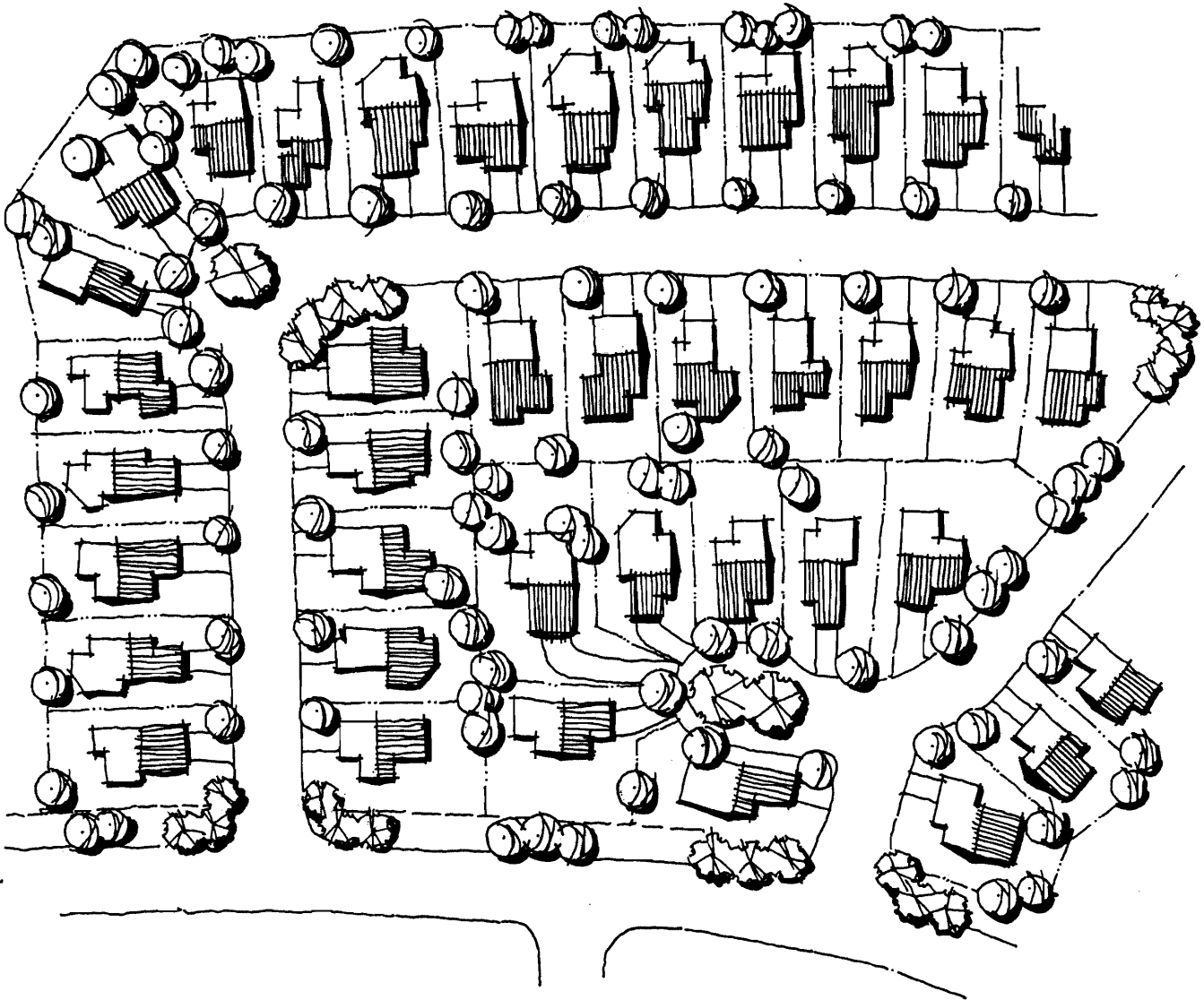


Z-lot configuration with shared sideyard enables density without concurrent loss of usable yard areas

Clustering



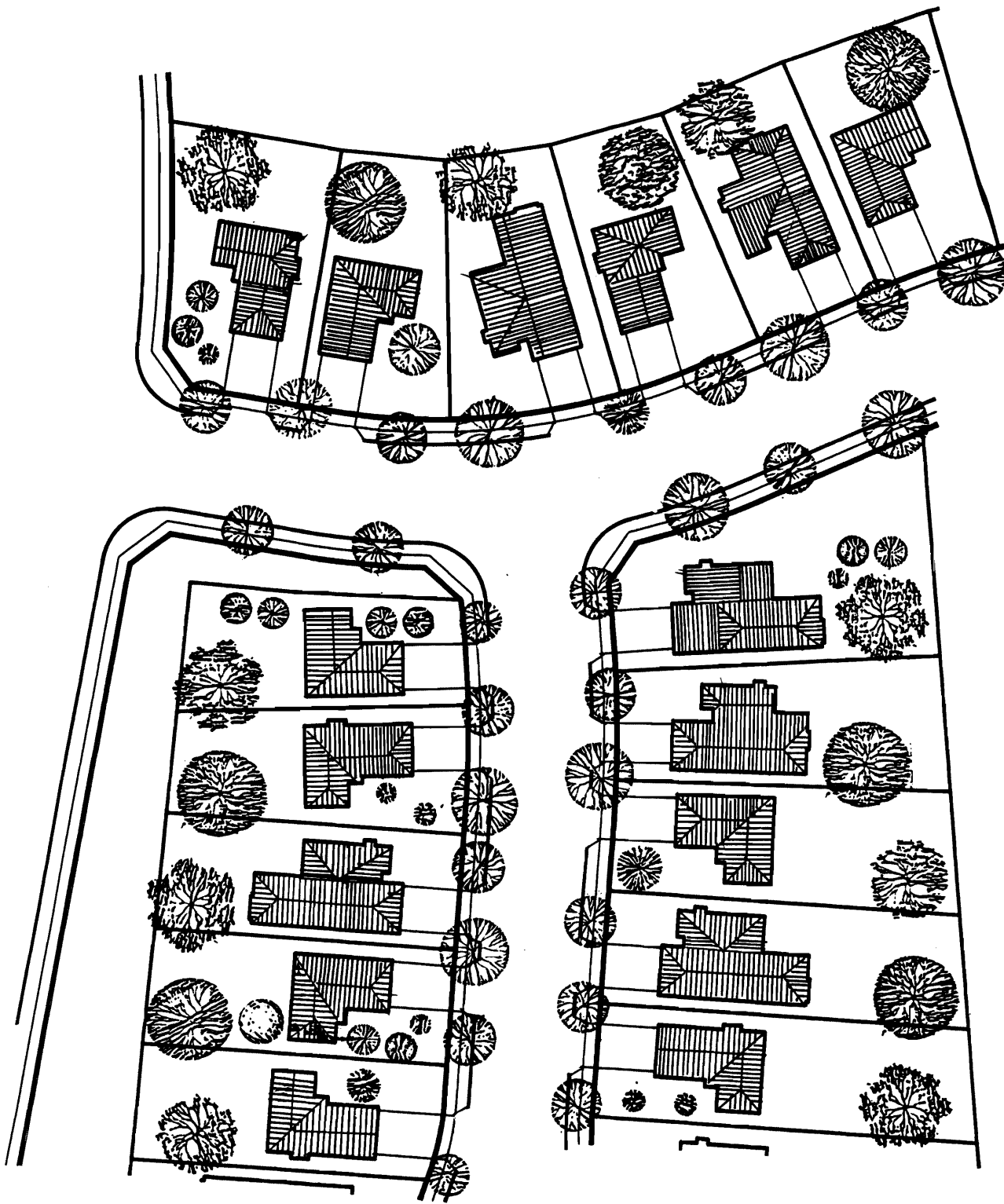
Clustered, detached units with common area allows development in topographically difficult areas, with a gain in recreational or dedicated open space



Similar sized lots on both sides of the street are encouraged, so that building setback and structure scale are consistent. Clearly defined and visible entryways that orient to the street are preferred. The lot layout should encourage public activity in the public realm and welcome visitors from the on-street parking.

b. Specifications for Appropriate Lot Sizing

In Section B. Planning Area Development Standards of this Specific Plan, the descriptive summary and planning standards discussion for each of the seven residential planning areas sets forth the lot size regulations applicable to each planning area. Figures IV-49 and IV-50 illustrate examples of lot sizes and relationships.



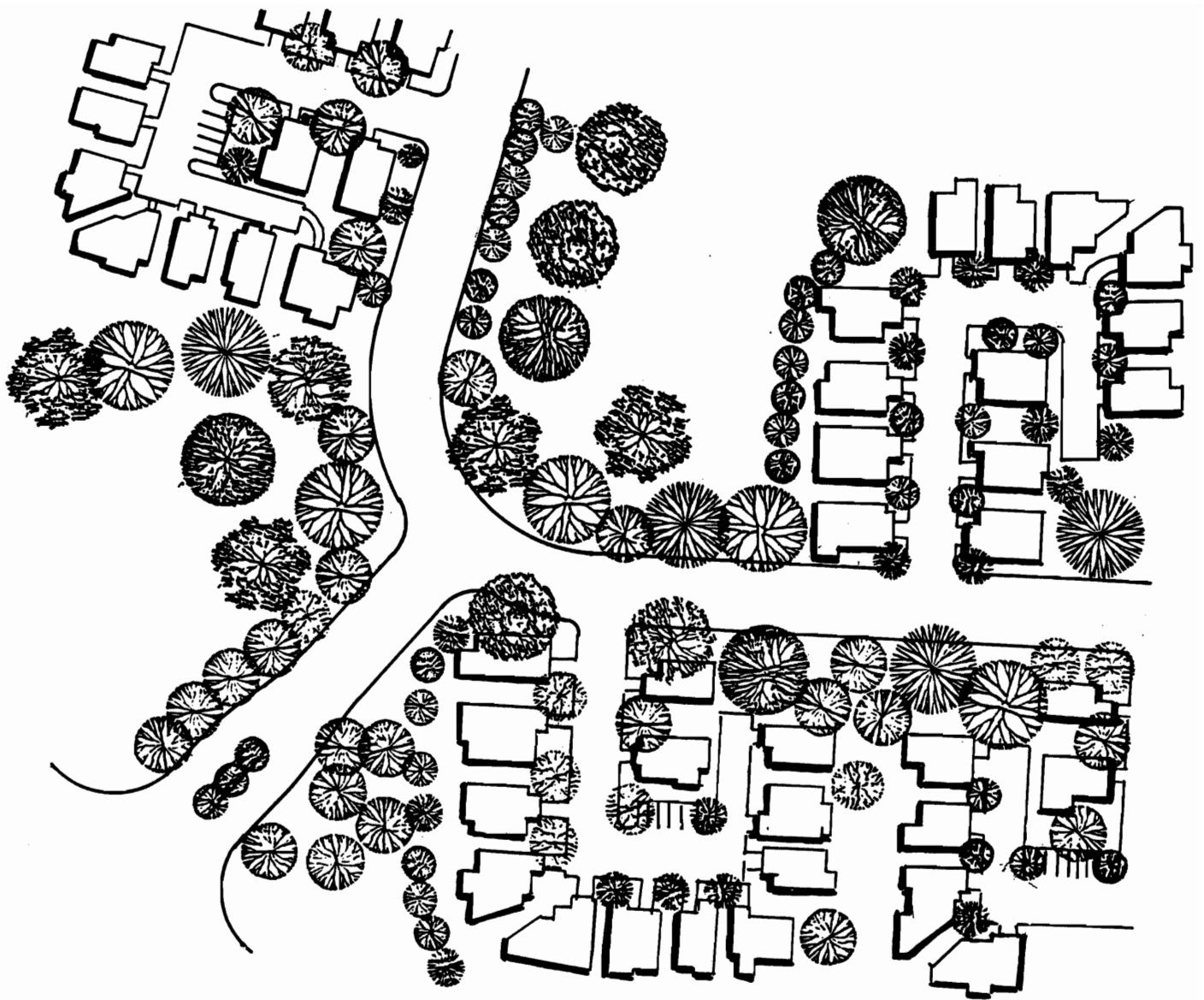
THE CROSSROADS IN WINCHESTER SPECIFIC PLAN

 Lohr + Associates Inc

FIGURE

MEDIUM DENSITY
RESIDENTIAL
DEVELOPMENT
EXAMPLE

IV-49



THE CROSSROADS IN WINCHESTER SPECIFIC PLAN

 **Lohr + Associates Inc**

LOW OR MEDIUM
OPTIONAL CLUSTER
DEVELOPMENT

FIGURE

IV-50

c. Siting

Appropriate

- Plan to maximize open space areas and to preserve on-site natural features
- take advantage of natural and man-made amenities (views, parks)
- Cluster of homes to create open space pockets

Inappropriate

- Disregard for on-site natural features
- Disregard existing topography

3. Building Massing

a. Proportion and Siting

The proportion and siting of residential structures do much to influence the character of a street, as well as neighborhood. Building massing and placement in The Crossroads Specific Plan should strive to be interesting, inviting, and functional. Building setbacks from public streets should be minimized. "Build-to" lines should reflect the desired character of the area and bring buildings close to the sidewalk.

Where possible, similar uses and building intensities are encouraged to be located on both sides of the street. Buildings should be placed to provide an open vista at intersections, and allow pedestrians to see the layout of the neighborhood.

Appropriate

- Land use changes separated by alley, paseo or other landmark
- Similar building scale to reinforce the character and identity of a street
- Varied garage and entry orientation of adjacent buildings to provide a variety along streetscape.
- Placement of detached garages at the rear property line
- Varied front setbacks as much as possible to give visual interest
- Varied architectural front elevations as much as possible to create interest
- Buildings on corner lots should have an increased side yard setback
- If one-story buildings are used they should be placed on corner lots or paired together on interior lots.

Inappropriate

- Use changes that occur midblock
- Monotonous and unarticulated building frontages
- Limited or restricted pedestrian paths
- Blank walls or unbroken series of garage doors
- No variety or setbacks on long straight streets
- Garages adjacent to street corners on corner lots
- Two-story structures with gable roofs adjacent to street corner
- Long linear vistas and building edges with uniform front setbacks.

b. Building Mass, Form and Elements

Elements of buildings (facades, rooflines and entries) should be varied and articulated to provide visual interest to pedestrians.

Appropriate

- Single-story elements at front setbacks and at street corners
- Sloping and stepped second-story volumes at the front and side yards along streets
- Articulated walls to create shadows and relief in the walls
- Projections and recesses
- Differentiated building materials
- Street level windows and entry
- A variety of plate height to create variation in massing
- Design elements that draw pedestrians in (covered entry porch or patio)
- An emphasis should be given to create units with a strong indoor/outdoor relationship (similar patio, overhead and flooring materials, extensive windows, etc.)
- Exposed rafters and raised banding at the eaves

Inappropriate

- Second-story volumes without any relief (such as stepbacks) in the front and side yards
- Plain walls without any relief or articulation

c. General Guidelines

Additional guidelines are given as broad design rules, intended to govern the overall appearance of the streetscene by recommending 'pedestrian-friendly' architectural elements.

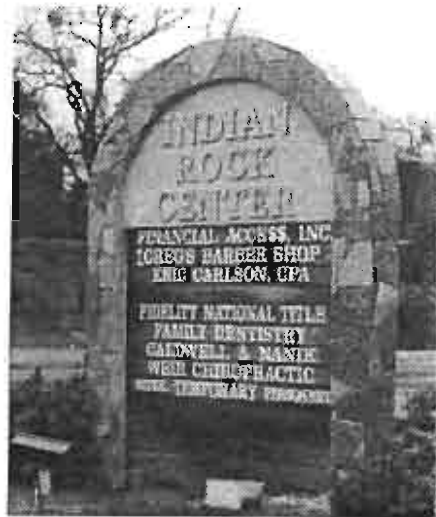
1. Random setbacks of landscaping should be incorporated in all structural design and unit siting.
2. Residential development shall include a mix of one and two-story dwelling units.
3. Buildings should be designed to an approximate human scale and should not appear to be monumental or monotonous. The use of the following design elements will help in creating buildings properly scaled to people:
 - a. Breaking up building masses into smaller, staggered masses;

- b. Breaking up long wall surfaces and roof lines into discontinuous surfaces;
 - c. Randomly textured materials on roofs and walls;
 - d. Extended roof overhangs.
4. The height and bulk of buildings should be appropriate to the size, shape and topography of the site and in harmony with its setting.
 5. Parking areas should be designed to facilitate both vehicular and pedestrian movements.
 6. The siting and design of structures within each planning area should consider the proper orientation to prevalent environmental conditions: sun, wind, terrain, views and vegetation.
 7. The siting and design of structures and landscaping should be sensitive to the modified terrain so as not to dominate the landform as seen from lower elevations.

D. Community Elements

Creating an atmosphere which is not only visually attractive at the human scale, but also physically functional in encouraging walking and biking, requires attention to the detail of the various community elements. These elements include, but are not limited to:

- Entry monumentation
- Community Walls and fencing
- Signage
- Sidewalks, bike paths, paseos and horsetrails
- Public area hardscape, such as benches, lighting,
- Bus stops



1. Form and Function

Control of form and function of community elements should focus on the items that are seen from the vehicle or seen and used from the pedestrian point of view. Examples are given below.

- Entry monumentation is primarily of use as identification to passing traffic. As such, its scale is specified for visibility from a distance. Vertical elements that are visible from long distances can also serve as landmarks, to help pedestrians orient themselves spatially within the community. They can also establish a tie to the historical significance of the community. The location of entry monumentation at the intersection of roadways means that pedestrians will encounter it continuously, and the design should reflect and reinforce the "pedestrian-friendly" intent of a TOD community. Finished hardscape with seating areas, congregation of shade trees oriented against the midday summer sun, textured mini-plazas and visual interest from rock and plant arrangements are some of the details which an appropriate entry monumentation will incorporate
- Sidewalks may be textured prior to (but not within the right-of-way of) intersections, or where a gradient is encountered to assist traction in wet weather
- Community wall and fence material guidelines are detailed in Section IV. Walls and fences should be utilized to enhance the completeness of the Specific Plan, not to isolate residents. Wherever possible, open wall design should be specified. Relief and articulated sections should be employed where long distances of a wall occur
- Lighting of urban arterials requires large, overhead light standards, but lighting of a pedestrian paseo should be accomplished with human-scale lamp posts of a sculptured nature, with multiple globes and a paint theme reflecting the community wall colors and/or texture
- Paseo openings should be accomplished with varying wall heights and landscaping, inviting to the pedestrian, and may offer a portion of wall at sitting height.
- Bus stops shall be integrated into the layout of the parkway. The sidewalk should widen, allowing for more pedestrian activity without impediment, closely placed street trees should provide additional shelter to the covered stop, and additional groundscape and low walls should allow the location of the bus stop to be seen from a reasonable distance both by driver pedestrian

- Architectural signage shall be consistent with the traditional style of The Crossroads. For more specific details, see Section IV. "Architectural Signage."
- Common areas, such as parks and plazas, should be organized with hardscape elements reflecting the materials used in the adjacent development. Low walls with capstone similar to the community wall, seating areas with overhead covering reflecting the construction and roofing materials of the surrounding neighborhood, benches, gazebos and even boulder arrangements in the hardscape should create a feeling of continuity with the residential and streetscape in place

2. Specific Element Guidelines

a. Outdoor Storage and Equipment Areas (Residential)

Appropriate

- Equipment shall be screened from view from any public street or adjacent public open space by landscaping, walls or fences
- Only electrical and telephone equipment are allowed to be placed within the side yard if properly screened.
- All screening materials shall use the same colors and textures that relate to the surrounding architectural styles

Inappropriate

- Storage areas, equipment and mechanical devices shall not be located within the front yard or side yard setback

b. Outdoor Storage, Equipment Areas and Loading Space (Commercial)

Appropriate

- All roof and ground-mounted equipment shall be screened from public view on all sides
- all ground level screening shall be composed of landscaping or constructed elements which are architecturally integrated with the building design
- Screening of roof-mounted equipment should be considered in the original design of the building and should, to the greatest extent feasible, be accomplished by strategic placement of equipment in combination with parapet walls

- Visual enclosure of roof equipment screening where visual overview from adjacent streets occurs

Inappropriate

- Roof mounted screens not integrated with the building architecture

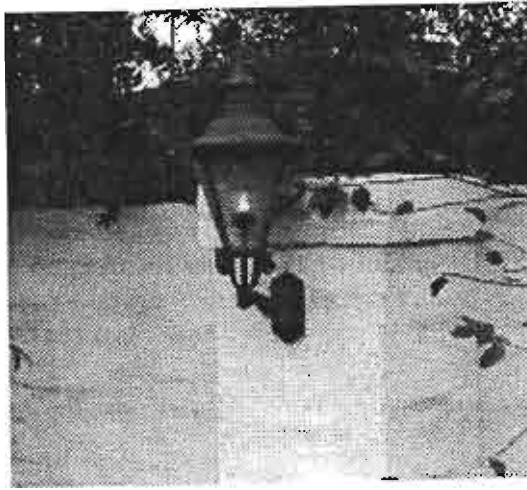
c. Signage

1. The identification and directional signs including the location, materials, colors, copy and the method of signing, size, and construction shall be approved by the County in accordance with the existing Sign Ordinance, except as noted herein.
2. Identification signs are restricted to advertising only the person or company located on the lot. Hours of operation and telephone numbers are prohibited. Moving or flashing lights are prohibited. Internally lit lighting is preferred.
3. All ground signs shall not be located closer than six feet to any property line.
4. All monument signs shall not exceed a height of ten feet measured vertically from the base at ground level to the apex of the sign.
5. The area of a directional sign may not exceed six square feet. Maximum height shall be four feet.
6. The design of permitted signs should be architecturally integrated with the building design.

d. Lighting

1. The design of light fixtures and their structural support shall be architecturally compatible with the surrounding buildings.
2. Light standards shall not exceed 35 feet in height.
3. All parking lot and driveway lighting should provide uniform illumination. Accent illumination is recommended at key points such as entrances, exits, loading zones, and drives.
4. Lighting should be shielded and situated so as to not cause glare or excessive light spillage on neighboring sites.

5. Lighting components shall be designed to comply with Ordinance No. 655, Mt. Palomar Observatory Policies, as applicable.



E. Commercial

1. *Architectural Styles - Commercial.* The commercial style guidelines will include the same as those utilized for the residential, because the site size is appropriate to the scale of residential homes. Concrete tilt-up is acceptable, should there be a large anchor tenant which requires roof spans in a building exceeding the efficiency of wood framing, but the exterior finish must be consistent with the attached, in-line stores and the overall theme of the center.

The builder should focus on details, offsetting walls and rooflines to provide visual interest, scaling the entry to the human scale of the shopper, shielding against summer sun with awning or overhead structure, incorporate trellis, or pergola or colonnades with open roof of cross rafters, to invite walking from parking or between stores. Entry to buildings should always be clearly visible and differentiated from the rest of the storefront.

Requirements for parking landscape are, at minimum, a landscaped finger with shade tree every 15 stalls, with landscaped edge three feet in width fronting all parking areas; landscaped berm of minimal height of 30 inches shall shield parking areas from visibility from the street; commercial trash enclosures and trashcan enclosures, recycling centers, art/sculpture in industrial. In retail, look for details that create human scale: benches, textured sidewalks, decorative lighting, cupolas or clocktowers, fountains, child areas; build up a theme of neighborhood center which cannot be duplicated by the large power center.

2. *Materials.* Quality materials, which excludes corrugated metal or plastic, wire fencing except in industrial or school applications, bare or exposed block, plywood fencing. Materials should include examples of well done country and rural, including river rock, brick, craftsman wood, rock and iron tubing, split rail, plant-ons, copper in some roof applications.

3. Architectural Review

It is the intent of this section that the project master developer (sole owner of land within the Specific Plan or authorized representative of such land) or, in lieu of a master developer, the developer or builder of a subdivision, establish the architectural theme and style of each neighborhood.

Further, it is the intent of these guidelines that the developer(s) or builder(s) of the community shall have the design freedom to respond to market demand for size, character, and other variables of the housing industry.

a. Timing of Architectural Presentation

A master developer may submit to the Planning Department architectural guidelines for the entire project, or for individual subdivisions, at any time prior to the submittal of subdivision maps and while it still retains control of the land, as it deems appropriate. In lieu of submittal of architectural guidelines by a master developer, the developer or builder submitting a parcel map for subdivision of a large parcel shall submit, at the same time, guidelines for standards outlining the architectural style, treatments and materials consistent with the theme for that subdivision.

b. Architectural Review

The project master developer shall have the responsibility to review submittals of architectural style for subdivisions, to ensure compliance with the intent of existing guidelines. In lieu of the presence of a master developer, the developer or builder submitting its subdivision map shall have the responsibility to identify the overall design theme for its project and show compliance with the guidelines established at the time of submittal; or the developer(s) or builder(s) may choose to establish an architectural review board and endow it with the authority and responsibility to ensure such compliance.